

ONEONTA

Downtown Main Street

Streetscape Assessment

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Prepared For:

**City of Oneonta
Otsego County, New York**

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Guide to this Document

This assessment is organized into the following sections:

Introduction

Describes the history of the Main Street Streetscape, purpose and scope of this document.

Existing Conditions

Describes and documents the existing conditions within the public realm of Main Street including site amenities, historic fabric, signage, vegetation, access, etc.

Design Recommendations

Describes improvements and opportunities for Main Street's downtown core based on community input and professional recommendations.

Implementation & Phasing

Identifies a series of steps and priorities in which the recommended streetscape improvements could be implemented, and identifies short and long-term projects and potential funding sources.



Project Context: Downtown Oneonta, New York



Project Area- Main St.
(Chestnut - Elm)

INTRODUCTION

History of Main Street Streetscape

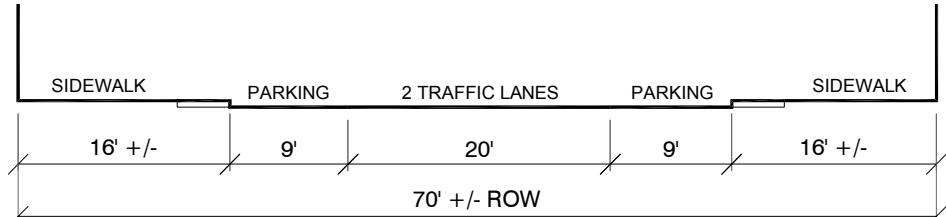
Main Street began as a footpath used by the Native Americans. Following the American Revolution and by the late 1700's, settlement began in the area. Oneonta's population grew slowly throughout the early 19th century, with agriculture the leading cause for growth. Following the Civil War and the inception of the railroad and later the trolley system (1888-1971) Oneonta grew rapidly. Education has also been an important part of the growth of Oneonta - with the emergence of SUNY College at Oneonta (1880's) and Hartwick College (1920's).

Main Street developed as the commercial center of Oneonta and experienced much change entering the 20th century. Main Street was paved in 1902 with bricks, replacing the wooden blocks laid in 1889.

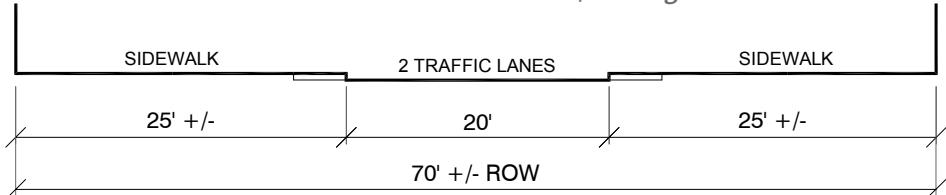
Sources: Greater Oneonta Historical Society webpage & "Early Oneonta: A Pictorial History" by Robert Jackson and John Harther

Main Street Today

The existing streetscape design dates back to 1976 (constructed 1978-1980). "The original traffic calming features of the early design have remained and are now an integral part of the 'streetscape' "(Small Town Traffic Calming). Curb extensions/bump-outs are located at each street corner and most mid-blocks; these features have proven to slow traffic and reduce pedestrian crossing distance and safety. Bump-outs provide the additional benefit of expanded sidewalk space for seating and amenities. The 70 ft right-of-way from building face to building face includes (on both sides of the street): 10'+/- sidewalk width, 4'-9" snow storage, curb and protected on-street parking. The street has two traffic lanes.



Main Street Cross Section w/ Parking



Main Street Cross Section w/ Curbed Bump-outs



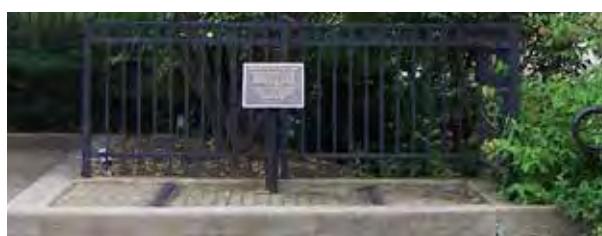
Main Street Oneonta, New York

EXISTING CONDITIONS

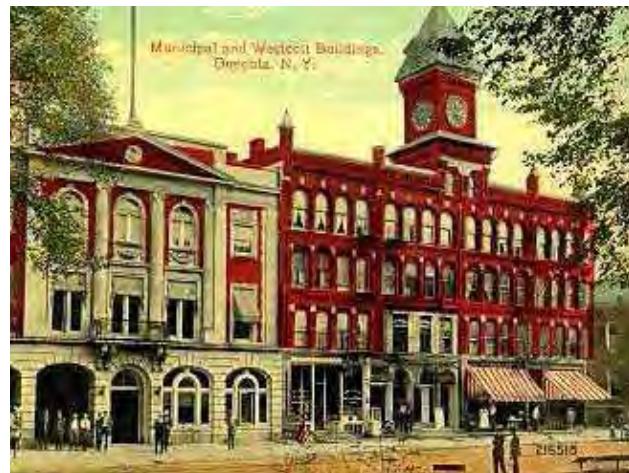
Introduction

This section summarizes the site investigations into a series of assessments of existing conditions. These assessments inform the recommendations identified later in this report. The following assessments are based on a combination of:

- on-site field investigation (March 1, 2013 & July 22, 2013)
- site walk-through with the client (July 22, 2013)
- Steering Committee Meeting (July 22, 2013) and
- desktop analysis of base information provided by the client (DPW).



Clock tower @ Municipal Building (top left); Building Materials (top right); Trolley Memorial (bottom);



Oneonta Municipal Building



Main Street Looking East (top); Windsor Hotel (bottom left); Oneonta Hotel (bottom right)

Historical/Cultural Fabric

Many glimpses of the historic fabric remain along the Main Street streetscape, including the Greater Oneonta Historical Society, which is located at 183 Main Street (corner of Dietz St. and Main St.) Some historic elements found within the corridor include:

- Clock Tower
- Historic building facades
- Memorial to Trolley
- Hotel Oneonta

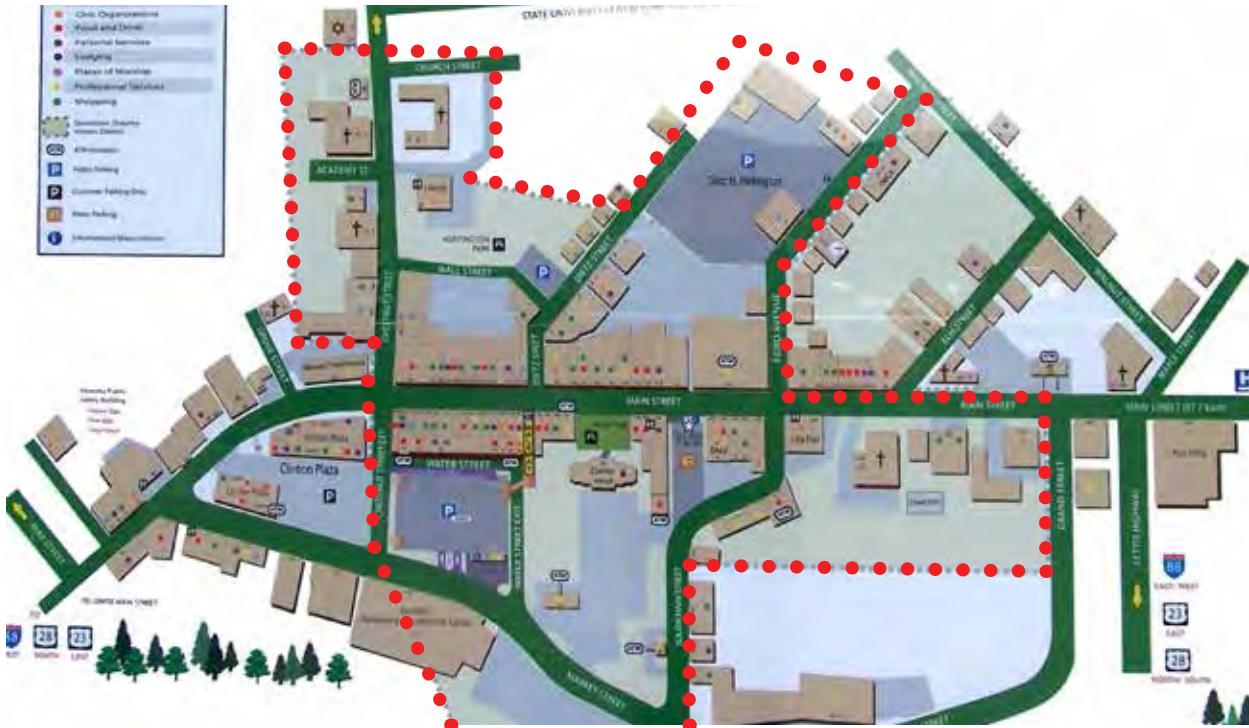
In general, the variety of streetscape amenities added to Main Street over time have little historical relevance but have been assumed into the modern day image of downtown.



Main Street Looking West (top & bottom photo)

Gateway

It is unclear where “downtown” begins along Main Street. Within the assessed area, three minor “gateways” were noted. These include:



DOWNTOWN HISTORIC DISTRICT

“Oneonta Downtown Historic District encompasses 64 contributing buildings and one contributing site. It encompasses the city’s intact commercial and civic core and includes commercial buildings, six churches, the city’s historic civic buildings, a few industrial buildings, and a small park. The district includes several separately listed buildings: the Masonic Temple, Old Post Office, Municipal Building, Ford Block, and Oneonta Theatre.”

1. MAIN ST./CHESTNUT ST. (from W)

The roadway straightens at Chestnut Street and building faces becomes more intact. Chestnut Street connects directly to Market Street, Water Street and the Municipal Parking Garage.

2. MAIN ST./ELM ST. (from E)

The visual character and scale of Main Street changes noticeably at Elm Street. The intersection is also skewed and more expansive.

3. DEITZ STREET

Deitz Street terminates into Main Street. It connects to one of the City's larger parking lots and is utilized by the public to access Main Street. A large planter cluster and bump out are dominant in the commuters view.

Opportunity exists to enhance all three intersections to announce this transition into Downtown.



Main Street & Chestnut Street Intersection



Main Street & Elm Street Intersection



Deitz Street & Main Street Intersection

Circulation



Parking



PEDESTRIAN/VEHICULAR CONFLICTS

Pedestrian circulation is generally fluid due to generous sidewalks and narrow street crossings. However a couple vehicular-pedestrian conflicts were noted. These include:

- Corner of Chestnut & Main Street
The signal sequencing appears confusing and deficient for pedestrians crossing the street.
- Alley Parking - No parking signs are ignored and cars have been observed exiting the alley into Main Street and using the apron to turn around.
- Larger planters provide a visual barrier at intersections, affecting site-lines for safe crossing.

PARKING STRUCTURES & LOTS

There are a number of parking opportunities in the vicinity of Main Street.

These include:

- Municipal Parking Garage
Located off Market Street and accessible from Main Street via a walkway (south side of street, midblock)
- Westcott Parking Lot
Located off Market Street and South Main Street. Parking is metered by the city.
- Clinton Street Parking Lot
- Deitz Street Parking Lot
- Wall Street Parking Lot

ON-STREET PARKING

Parking is protected by curb bump outs the length of Main Street. Due to the limited width of the street, the current design nearly maximizes parking.

Opportunities to expand on-street parking include:

- Remove unnecessary loading zones
- Relocate fire hydrants
- Remove bus stop

BUS STOPS

Currently there is one bus stop along Main Street. There is no bus shelter or seating at this bus stop.

Crosswalks



Crossing distance is shortened by curb bump-outs. Crosswalk locations are noted by curb ramps and are marked with a ladder pattern, painted the width of the crossing. Paint has faded and should be restored. Curb ramps should be upgraded to meet current ADA standards with tactile warning surfaces. Additional standard features should be considered in coordination with the NYSDOT regional office.

MID-BLOCK CROSSING

There are no mid-block crossings; an opportunity exists to consider one opposite the Plaza.

PEDESTRIAN SIGNAL POLES

Signal poles are limited at crosswalks. This inconsistency should be upgraded for all intersections. Signal poles with push buttons are currently located in both directions at Chestnut, South Main Street/Ford Ave.

Signaling for crossing only Main Street is provided at Dietz St.

No signaling is provided at Elm Street.

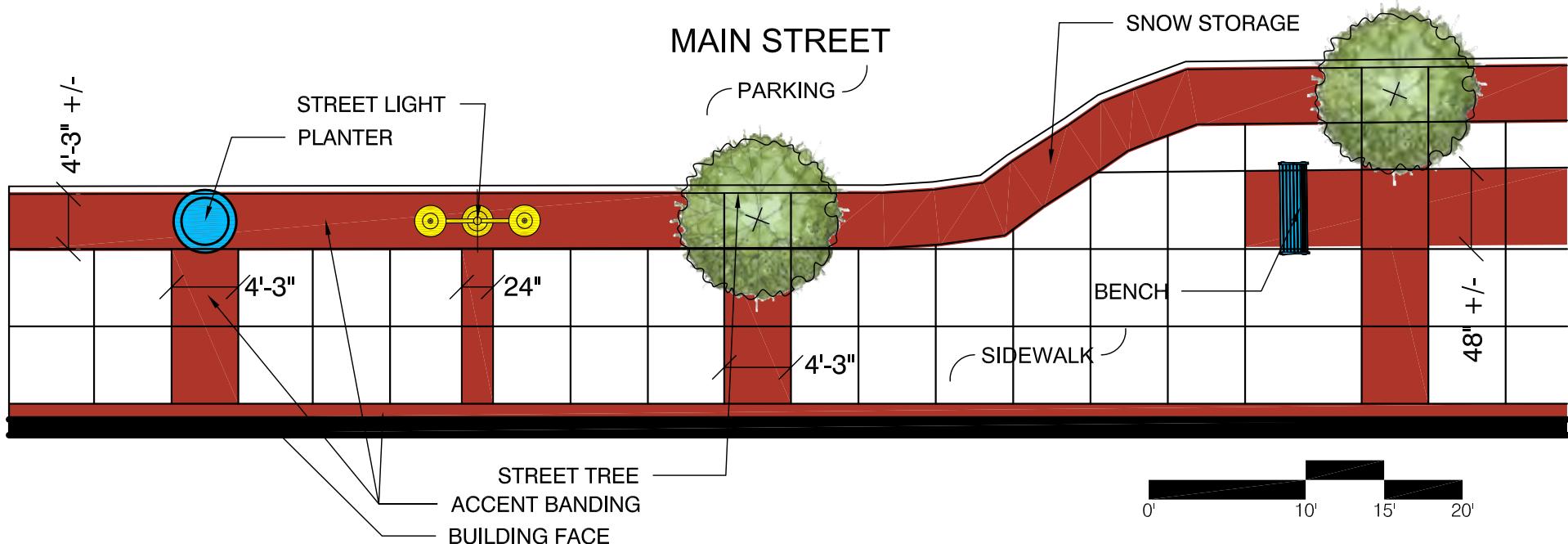
Design style of pedestrian lights are not consistent.

Special Needs/Accessibility

There are no existing facilities along Main Street serving those with disabilities/special needs (visually and hearing impaired).

Alley crossing should be designated along the sidewalk path with physical markings and signage.

Paving



The streetscape has a varied palette of paving materials.

BRICK ACCENTS

Brick pavement accents repeat at every light pole (24"), tree (48"), planter (48") and seating area. A "stone" textured band (patterned concrete) and/or brick band runs parallel to the building face.

Brick accent banding remains on the North side of the street. Banding on

the South has been more recently replaced with patterned concrete. Although largely intact, brick pavements are showing wear and should be selectively removed and/or repaired where appropriate. Pavers could be salvaged for reuse in areas where damage remains minimal. Consideration should also be given to unify pavement types and define use areas through good urban design. Current brick pavers are manufactured with greater durability.

SNOW STORAGE

A combination of concrete pavers (north side) and patterned concrete (south side) was noted. Where pavers have been used, sediment is evident immediately behind the curb and at concrete joints due to settlement. Weed growth is typical to be found here.

Some drainage problems present a hazard due to accumulated runoff that can become slippery if not removed during

cold weather months.

Tree root crowns at mature street trees have bowed pavements so that some snow storage areas have become uneven.

Some brick and concrete paver pavements are in disrepair: chipped, cracked, heaving, etc. Patterned concrete has faded and lost its original aesthetic appeal.

CONCRETE SIDEWALKS

Overall the concrete sidewalks are in very good condition. Some slabs have settled, which has created lippage and minor trip hazards. Where settlement is

excessive, selective slabs should be removed and replaced or the exposed edge ground to a maximum 1/4" lip or less. Pavements should be cleaned to remove gum, sediment buildup and debris in joints.



Site Amenities/Furnishings



Since the 1970's Main Street has seen the addition of unrelated amenities. This has resulted in an eclectic, mismatched composition of furnishings which lack historical significance and relevance to downtown. Furnishings are generally outdated and showing wear. Maintenance concerns include chipping paint, spalled concrete, etc.

TRASH RECEPTACLES

Receptacle spacing seems both logical and adequate the length of Main Street; they are generally located at crossings and mid-block.

Wood trash and recycling receptacles should be updated. Newer model (stone aggregate) installed

to match planters, although durable, seem outdated and have little historical relevance. They also require DPW staff to lift containers up from top vs side door.

BIKE BOLLARDS

Two basic bike bollard styles are used along Main Street. Both lack ornamentation but could be retrofitted. They are generally located at curb bumpouts, separated from the main flow of traffic. Bike bollard locations should be expanded to encourage bicycle use.

NEWSPAPER VENDOR BOXES

Newspaper vendor boxes are concentrated together primarily at street corners and mid-block. A small number appear to be abandoned or not refilled regularly.



PLANTERS/POTS

The circular planters are original to Main Street's 1978 restoration. They have a sandblasted finish. Most planters are vegetated with annuals. Some have small tree species. Annual plantings are paid for by the Garden Club and maintained by the City. Most exhibit signs of deterioration. Minor spalling/scaling is visible on most planters. A small percentage are in major disrepair (concrete flaking and wire mesh visible). Planters look to be installed prior to sidewalks.

Planters to remain can be rejuvenated by cleaning, repair or application of a veneer product. A light sandblasting may rejuvenate. Repair of surfaces can be done with a polymer based overlay.

A large brick circular planter at Elm Street has a new precast cap. The mass and character of this planter seems to visually fit the space.



ORNAMENTAL IRON WORK & RAILINGS

Design character of the existing railings relate well to octagonal light fixtures. New LED's will be acorn shaped. Nonetheless, the existing custom railings remain in good condition.

Railing is repeated throughout Main Street corridor.

BENCHES

The original design located benches at all bump-outs. Some benches have been removed. These should be replaced.

Benches typically face up and down the



sidewalk,. Opposite the Alley benches face toward the street. All benches are cast iron framing with wood slats. Wood slates in disrepair need to be painted or replaced. If replaced, composite materials should be considered to reduce maintenance.

OTHER SEATING

Planters are used as informal seating opportunities. Many stores, cafes and restaurants offer movable tables and chairs.



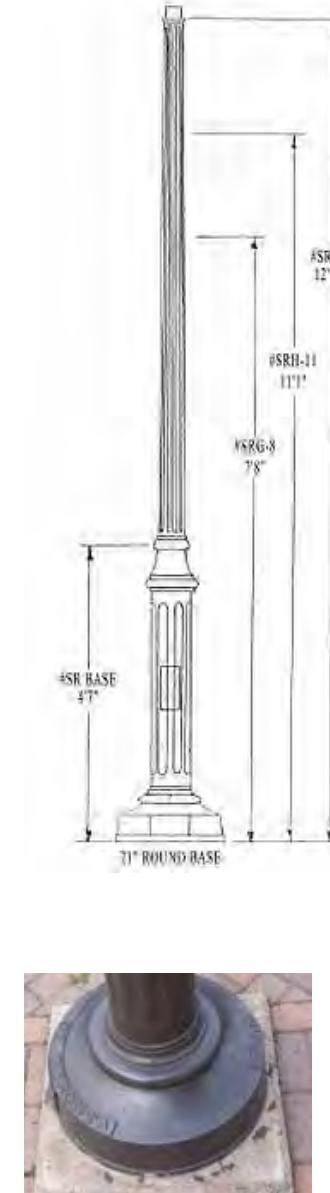
Lighting

STREET & SIDEWALK LIGHTING

Existing light poles are spaced regularly (50'-75') along Main Street. All ornamental light poles are pedestrian scale (height). Additional illumination is not present at intersections. Some facade lighting contributes to sidewalk illumination.

Current poles are original to the 1976 streetscape design. This design introduced the octagon pole shape, which is not known to be a contributing historic elements. Poles are accessorized with: (2) fixture arms, (2) planter arms and (2) flag pole mounts. Poles have been recently updated with banners, mounted to poles with pipe clamps.

Most recently, the City has purchased LED Caged Acorn replacement fixtures (by Sternberg). These will replace the existing fixtures. Head style was selected based on historical precedence and cost efficiency. Head style is original to the street.

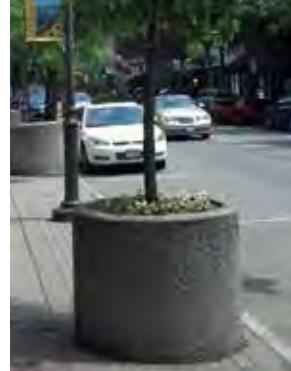


HERWIG POSTS #SR-SERIES

MATERIAL:
CAST ALUMINUM MIN. .08" METAL
BLACK FINISH
MID HND HOLE 3 1/2"X 8"
BASE HND HOLE 3 1/2"X 5 1/2"
INTERIOR BOLT DOWN 4-5/8"X 18"
PLATED ANCHOR BOLTS, NUTS &
WASHERS

OPTIONS:
SINGLE WEATHERPROOF RECEPTICAL
ANTIQUE OR CUSTOM COLORS
4 1/2" O.D. STRAIGHT ALUMINUM
SHAFT W. 3" O.D. TONON, .237 OR
.337 WALL, 4 1/2" TO 3" O.D. TAPERED
SPUN ALUMINUM SHAFT, .188 WALL

Landscape Elements



FREESTANDING PLANTERS

Planters are planted annually by the Garden Club and maintained by the City. Due to excessive shade, vandalism and/or insufficient water, vegetation in many planters is under stress.

Movable planters provided to store owners all appear in good condition.

Trees in concrete planters are stressed and have reached maturity; trees should

be removed.

STREET TREES

Tree species along Main Street are limited to Honeylocust (*Gleditsia triacanthos*), Hackberry (*Celtis spp.*), Tree lilac (*Syringa reticulata spp.*) and Norway Maple (*Acer platanoides*). Overall, honeylocust spp. are in good health; many have reached a mature size and provide welcomed shade to the sidewalk environment. Some younger re-

placement trees were noted. Some tree pits have been paved over.

Generally trees are located in tree pits, with the exception of a few smaller species in raised planters. Trees are spaced consistently between Chestnut and Ford but become more sporadic to Elm. It is unclear if the presence of utilities and sidewalk vaults have limited tree plantings.



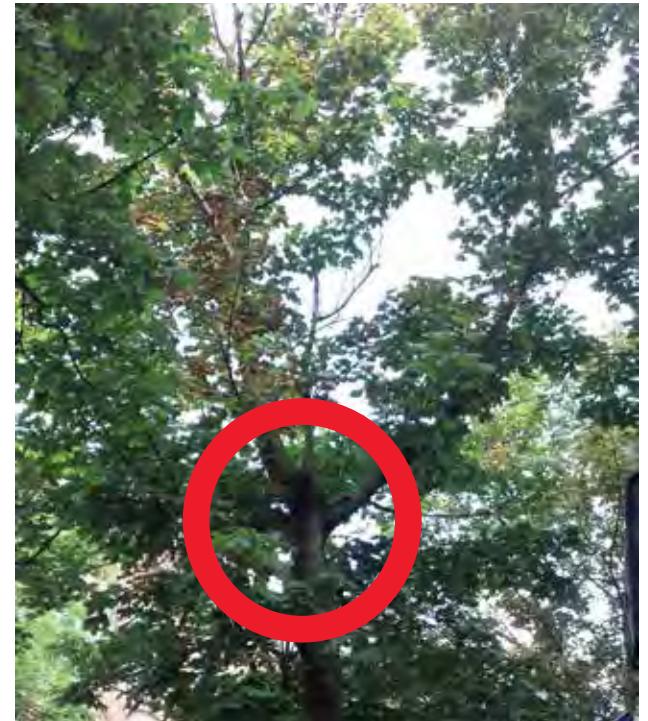
ROOT FLARE

The root flair on some mature Honeylocust trees has become extremely large and displaced surrounding pavements.



DIE BACK

Tree has been topped. Die back and tree stress is visible.



TOPPED

Existing Norway Maples are beginning to show stress; main leaders have been topped and branching is starting to brown, likely due to insufficient space for roots. Additionally, in New York State, Norway Maples are listed as an invasive species. Tree species should be selectively removed and replaced along the street corridor.

Wayfinding, Information & Identity

Existing signage along Main Street, for the most part, is uncoordinated.

IDENTITY/WELCOME SIGNAGE

Existing identity signage, iconic in nature, was not found. There was nothing noted that would let the visitor know they have arrived at a particular destination. No character-defining signage.

INFORMATION/WAYFINDING/DIRECTIONAL SIGNAGE

Pedestrian-oriented wayfinding signs are very limited and difficult to locate. Only two downtown area maps were noted within the assessed corridor that provided detailed information to the visitor (Market Street/Municipal Parking Garage access & Westcott Public Parking Lot).

CITY SLOGAN

Conflicting messages: "Life Enjoyed" vs. "City of the Hills."

"Ford Plaza"
or "Muller Plaza"



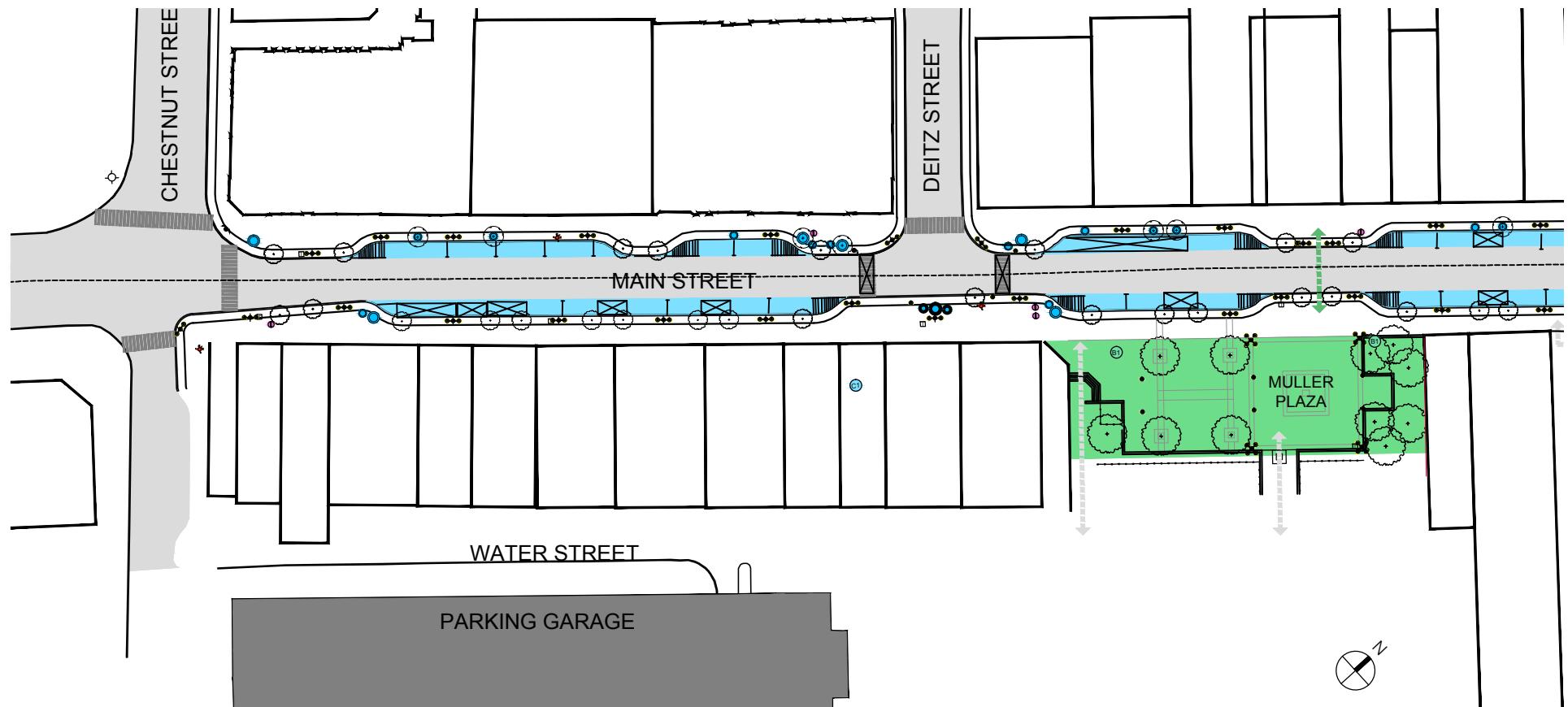


MEMORIAL

Memorial dedicated in 2001. It features a small plaque and piece of original track commemorating Oneonta's Trolley Car System that ran through this location. Paving detail extends line into pavement.



Existing Conditions Plan



EXISTING STREETSCAPE ELEMENTS

- Street Light (w/ 1 head)
- Street Light (w/ 2 heads)
- Street Light (w/ 4 heads)
- Traffic Light
- Crossing signal
- Existing Tree
- Existing Shrubs/Per.
- Empty/Infilled Tree Pit
- Planter
- Tree in Planter
- Bike Rack
- News/Mag. Rack
- Trash Receptacle

EXISTING HISTORIC FABRIC

- Recycling Receptacle
- Postal Box
- Bench
- Bus Stop
- Crosswalk

EXISTING UTILITIES

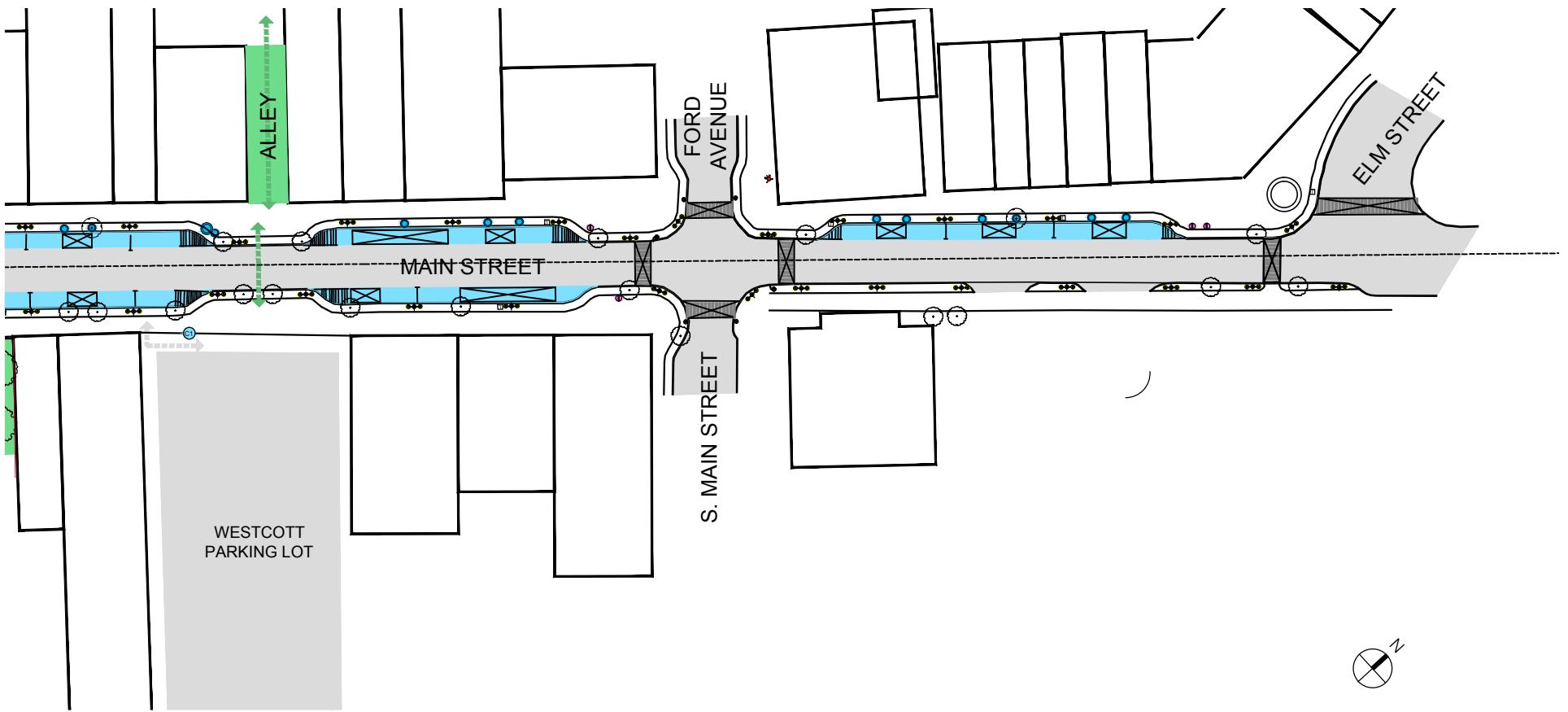
- Fire Hydrant

EXISTING SIGNAGE

- Community Identifier
- Vehicular Directional
- Pedestrian Directional
- Informational/Area Map

OPEN SPACE & OPPORTUNITIES

- Existing Pedestrian Passage
- Pedestrian Passage Opportunity
- Existing Open Space
- On street Parking
- Surface Parking
- Parking Garage



EXISTING STREETSCAPE ELEMENTS

- Street Light (w/ 1 head)
- Existing Tree
- R Recycling Receptacle
- Historic Building
- Street Light (w/ 2 heads)
- Existing Shrubs/Per.
- P Postal Box
- Community Identifier
- Street Light (w/ 4 heads)
- Empty/Infilled Tree Pit
- Bench
- B1 Vehicular Directional
- Traffic Light
- Planter
- Bus Stop
- B2 Pedestrian Directional
- Crossing signal
- Tree in Planter
- Crosswalk
- C1 Informational/Area Map
- News/Mag. Rack
- Trash Receptacle

EXISTING HISTORIC FABRIC

EXISTING UTILITIES

- Fire Hydrant

EXISTING SIGNAGE

- Vehicular Directional
- Pedestrian Directional
- Informational/Area Map

OPEN SPACE & OPPORTUNITIES

- ↔ Existing Pedestrian Passage
- ↔ Pedestrian Passage Opportunity
- Existing Open Space
- On street Parking
- Surface Parking
- Parking Garage

DESIGN RECOMMENDATIONS

Introduction

A primary objective of this plan is to enhance the existing design character of the street corridor with sustainable pavements, plantings and urban amenities. Materials must be used that have historical relevance to the downtown. These include masonry elements such as clay brick, limestone, granite and bluestone; and decorative metal for railings and other amenities.

A secondary objectives would be to improve wayfinding and signage to provide a UNIQUE sense of place for Oneonta. Amenities can be retrofitted to provide character to the streetscape and reinforce

Design Recommendations

ROADWAY PAVING

Decorative enhancements can be made to crosswalks and key intersections. Main Street should be updated to meet current ADA standards and to address the needs of those who are visually or hearing impaired.

SIDEWALK PAVING

Pavement palette should be unified. Decorative enhancements can be made to designate seating areas and snow storage.

FURNISHINGS

A number of amenities have been incorporated into the design recommendations. These include seatwalls, new

curbed, raised and freestanding planters, benches, bollards and trash receptacles. Existing bike racks should be upgraded with ornamentation.

PLANTERS

Existing planters should be enlarged, replaced and/or repaired and improved.

STREET TREES & PLANTINGS

A varied palette of tree species should be introduced along Main Street. Gaps in shade along the street corridor should be infilled. Understory vegetation should be simple, low growing and provide seasonal interest. Plantings should be a combination of shrubs, perennials and annuals.

IDENTITY, WAYFINDING & INFORMATION

Identity, wayfinding and information should be provided through use of a family of signs designed to create a sense of place and identity for Oneonta.

STRUCTURE SOILS & STORMWATER MANAGEMENT CONSIDERATIONS

Structural soils should be considered to sustain existing and new trees where pavement limitations exist. Stormwater management is mandated by NYS and solutions should be evaluated for their practicality along Main Street.

DESIGN CONCEPTS

Add text....

Roadway Paving- Crosswalks

Crosswalks are considered an extension of the sidewalk, where pedestrians can legally and safely cross city streets. In the streetscape environment, crosswalks serve an important function by both physically and visually linking opposing sides of the street. AADAG, Traffic Control Devices and AASHTO provide guidelines for marked crosswalks, as well as standards for crossing improvements. These standards, combined with professional judgment and traffic engineering on a case-by-case basis should be consulted when redesigning.

SPECIAL NEEDS/ADA ACCESSIBILITY

In general, crosswalks can be improved to be universally accessible. Curb ramps must incorporate a detectable warning surface; this consists of small truncated domes built in or applied to a walking surface. This surface is detectable underfoot or by a cane and indicates the boundary to a vehicular route.

Pushbuttons, tactile arrows and the location of speakers must also be carefully engineered to provide accessible and usable information to pedestrians with disabilities. The DPW will need to weigh in on a consistent standard for Main Street.

GENERAL PEDESTRIAN SAFETY

Warning signs should be added to improve the general safety to pedestrians within the crosswalks. This can be accomplished by use of "Yield to Pedestrian" signs.

Another crosswalk technique, which can also "individualize" a crossing, is to raise the crosswalk area, a feature called a speed table. This technique increases the visibility of the crosswalk and pedestrians within it. It also functions as a "speed bump," slowing traffic.

DECORATIVE ENHANCEMENTS

Crosswalks can also be decorative, en-



hanced by using various materials and patterns. The following would enhance existing crossings:

- Striping
- Elastomeric inlays
- Pavers
- Integrally-colored & patterned-concrete
- Stamped Asphalt

MID-BLOCK CROSSINGS

There is opportunity to install a crossing mid-block, opposite the Muller Plaza. People are already inclined to cross here and as the plaza is programmed for more activities, this crossing will be used even more.



Sidewalk Paving

Sidewalk width remains generally consistent, ranging between 14-16', but greater at curb extensions. A minimum of 5' clear width should be maintained for passage of sidewalk users. Seating nodes can be further highlighted by decorative pavement.

CONCRETE SIDEWALKS

Patterned Colored Concrete

Sometimes referred to as stamped concrete, this technique allows the surface of concrete to be formed and patterned in a wide variety of textures and colors. This is not a historic product and applications should be limited on Main Street. Applications may include crosswalks or pavement band replacements. Limitations include color fading and inability to match if pavement has to be repaired.

UNIT PAVER SIDEWALKS

Unit paver sidewalks are constructed using small units laid to a pattern. There are many material options to choose from. Most common are clay, concrete and stone. Unit pavers come in many sizes, shapes, colors, textures and finishes. They are best used as accents in the streetscape (vs. the entire width of the sidewalk). They can be laid in many patterns using combinations of laying pat-

terns, colors, textures and finishes.

Clay Unit Pavers

High quality clay unit pavers tend to have limited variations of color, sizes & shapes. They have high compressive strength (10,000-12,000 psi) and low moisture absorption rating (4-5%). Can be prone to chipping and cracking under heavy loading. Have a high resistance to fading from UV light.

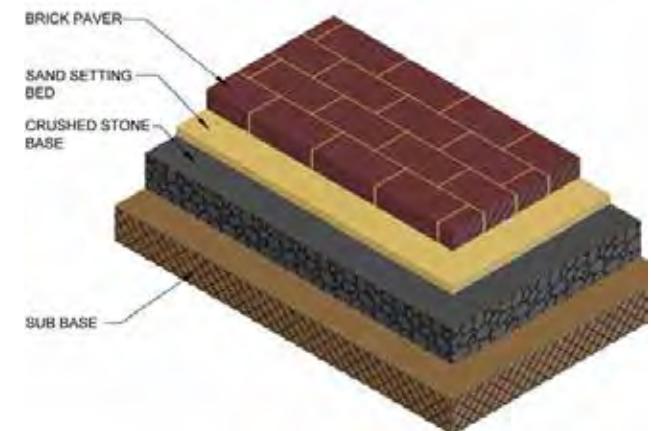
Concrete Unit Pavers

High quality concrete unit pavers tend to be manufactured in a large variation of color options, sizes & shapes. They have very high compressive strength (8,000 psi) and low moisture absorption ratings (4-5%) but tend to erode slightly over time due to extremes in weather. Concrete pavers will fade more readily from UV exposure than clay.

PAVING CONSTRUCTION

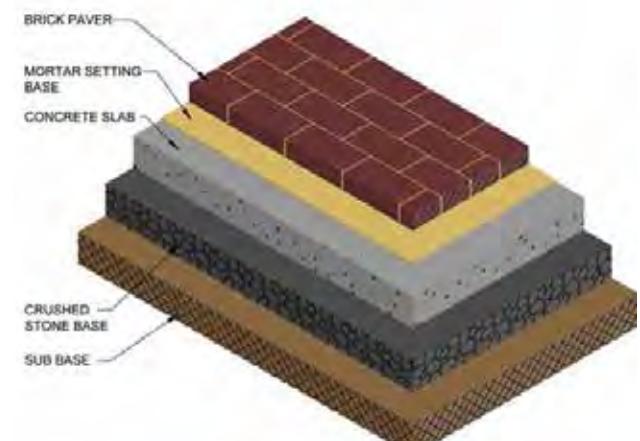
Flexible Base System

Pavers are placed on a sand setting bed and compacted stone base. Initial installation is less costly and repairs are easier. Pavement is subject to settlement, often seen behind curbs or adjacent to other pavement installations.



Rigid Base System

Pavers are placed on a mortar/bituminous setting bed and a concrete base. More costly for initial installation. Good for high traffic areas or areas subject to vehicular traffic. More resistant to settlement.



Furnishings

A design standard should be established for furnishings used in the public right-of-way. This will ensure consistency over time as new furnishings are added or replaced. Selections should be guided by a design professional and made by the City or an appointed committee.

STREET FURNITURE

Street furniture, including benches, tables and chairs, should be selected for their durability, maintenance, vandal protection and historical relevance. If existing



bench styles will remain, slats could be replaced with a composite material to reduce maintenance. A family of furnishings (varying length, with/without arms, etc.) should be used. Generally, furnishings should be placed at crossings and curb extensions and arranged in a variety of configurations to provide seating options. Backless benches and movable tables and chairs can be added to Muller Plaza.

INFORMAL SEATING

Informal seating opportunities can be created with free-standing walls; these also provide separation and protection from the street. Walls should be placed sidewalk-side of the snow storage strip.

TRASH RECEPTACLES

Trash receptacles should be selected for their durability, maintenance, vandal protection and historical relevance. Receptacles should be placed at crossings, outside eateries, near bench/table clusters and other logical locations along the street. Receptacles should be visible to ensure use and proper maintenance.

ORNAMENTAL BOLLARDS

Ornamental bollards can serve both a functional and ornamental purpose.

They can be used to protect streetscape elements or other built structures, to direct people to safe crossings or inhibit vehicular access to a certain location. For example, Elm Street is not a "T" intersection, so bollards could be used behind the curb line to direct pedestrians to the crosswalk. Bollards can also be used in confined spaces where there is no opportunity for planters.

ORNAMENTAL WICKET/FENCING

Ornamental wickets can add character and identity to a streetscape planting. When used in combination with tree pits or flush, curbed or raised planters, they also protect plantings from animals and people. Wickets can vary in height, but generally remain low, between 12-24". Due to their location and height, fence verticals should not extend above the top rail; this will prevent snags and injury.

TREE GRATES

Cast/ductile iron or plastic tree grates can be used around existing or newly planted street trees. Grates provide the benefit of expanded sidewalk space and need less maintenance than mulch. Grates can be cast in various patterns which allows customization. Grates must have breakout/removable rings to accommodate tree growth.

Planters

RAISED PLANTERS

Raised planters can come in a variety of shapes, sizes and finishes. When designing planters, consideration should be given to accessibility and visibility around them. Construction materials should be informed by the downtown character. Brick, bluestone and limestone are all prevalent to the area and downtown.

FLUSH PLANTERS

Flush planters have no curb and are placed at the same elevation of surrounding pavements. They can follow organic shapes created by curb extensions. They should be planted with intensive plantings such as groundcover, perennials, shrubs and trees. A small fence/railing may be used to protect plantings. Added space for planting can be gained by extending plant beds to back of curb. Perennial plantings should be used and fencing removed along the street where flush planters incorporate the snow storage area.

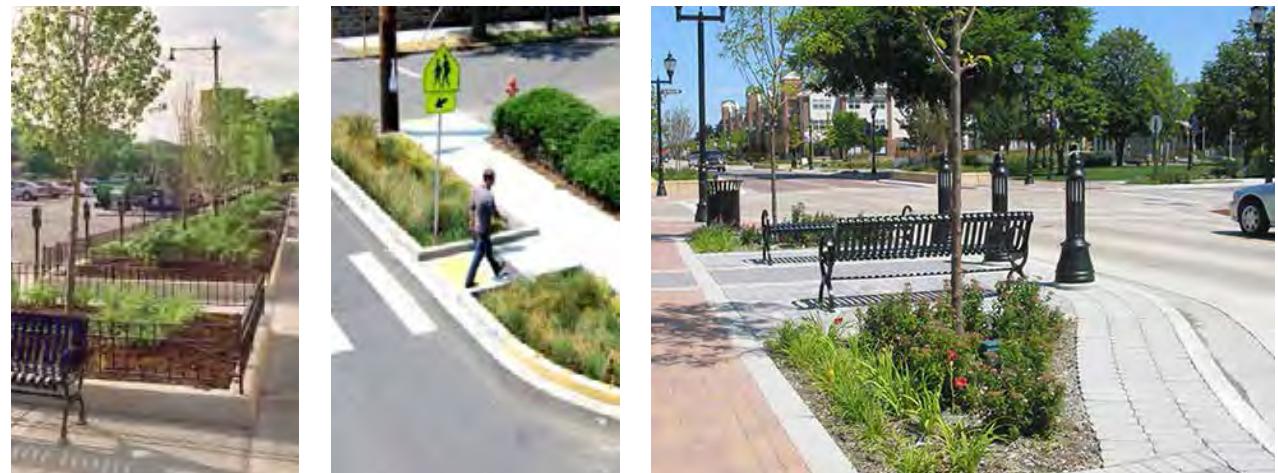
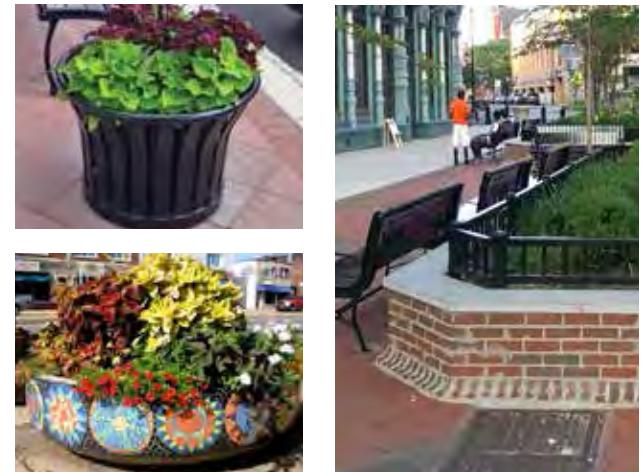
CURBED PLANTERS

Curbed planters can be cast-in-place concrete, granite, modular paver, or other material. Curb height is generally 4-8" and can come in a variety of profiles. Sidewalk drainage can be directed

to the planter or around, to the street.

FREESTANDING PLANTERS

Freestanding planters come prefabricated in all different shapes, sizes and colors. They are usually made of concrete or a synthetic material. They add color to the streetscape and are especially useful in tight spaces or where underground conditions prevent inground planters. When selecting planter(s), consideration should be given to vandalism and maintenance. Free-standing planters can be planted with annuals and sometimes a singular shrub or simple palette of perennials. Some of the existing planters could be cut shorter and repaired; local artists could be used to mosaic planters.



Planter Examples: Freestanding Planter (Top & Middle Left); Raised Planter with fence (Middle Right); Curbed Planter with Fence (Bottom Left) Flush planter without fence (Bottom Right);

Street Trees & Plantings

Street trees are a highly visible component of the streetscape. Because of foliage, bloom, branching, ability to be decorated, etc., they provide interest all seasons of the year. They offer shelter, shade, structure and rhythm to a streetscape. Trees and other streetscape plantings also improve air quality, reduce stormwater runoff, reduce the heat island effect, and provide other positive effects on the environment and people's lives.

Along Main Street, there is limited opportunity to expand plantings.

PLANT MATERIAL

Use and Effect

Control traffic, screen or enhance views, provide a background for an adjacent use or soften existing streetscape, the intended use and desired effect must be considered in the choice of plant material.

Plant Height & Sight Triangle

Attention must be given to plant height and visibility around plantings to ensure a safe streetscape environment. At intersections, crossings, driveways or alley conditions, plant material should not

extend over 30" from the top of pavement. Both pedestrians and motorists should remain visible within the sight triangle.

Plant Bed Preparation

Eliminate all vegetation, pavement and pavement base in any new areas to be planted. Sub-grade should be removed and replaced to min. 12" depth. Planting soil should be a specified amended soil for urban conditions. Apply an application of a broad-spectrum herbicide (Preen) before planting. The bed should then be fertilized and cultivated. After planting apply a 2- to 4-inch layer of mulch to the planting area to help prevent weed growth.

Plant Vitality

Success and vigor of the plant material is very important in providing year-round interest to streetscape plantings. Consideration should be given to sun/shade exposure, snow storage, drought tolerance and salt tolerance,

MAINTENANCE

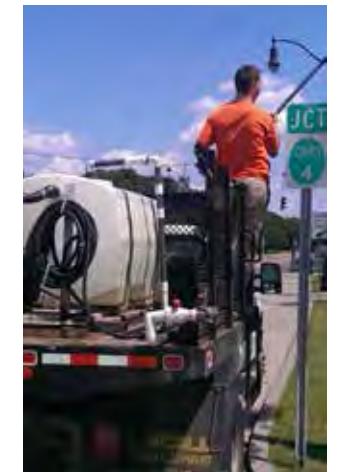
Consideration should be given to maintenance when designing new plantings. Maintenance includes weeding, watering, plant replacement, pest control and litter pickup. Who will be

responsible for ongoing maintenance? Will significant community commitment be expected? This information should inform the amount and type of plant material for a streetscape.

Landscape maintenance requirements and a schedule for both the public and private installations should be established by the City. See sample Maintenance Schedule (Appendix). Signed agreements by individuals committing to maintenance should also be considered.

Irrigation

Irrigation is one of the most important aspects of landscape maintenance. There are two options for irrigation, auto-



EXAMPLE SHRUB/PERENNIAL SPECIES LIST

matic or by hand. The latter is employed along Main Street; the City is responsible for irrigation for planters (planted by the Garden Club).

An efficient watering system should be adopted for maintenance purposes. City staff should be trained and watering system upgraded. Some cities have adopted a plan whereby city staff water the hanging baskets twice per week throughout the summer. Trucks transport water in large tanks (min. 500-1,000 gallons) and hand-held wands are typically used to water the flowers.



Daylily
Hemerocallis spp.



Sedum
Sedum spp.



Juniper
Juniperus spp.



Boxwood
Juniperus spp.



Roses
Rosa spp.



Salvia
Salvia spp.



**Ornamental
Grasses**



Purple Coneflower
Echinacea spp.

EXAMPLE STREET TREES SPECIES LIST

Street Tree Selection

Street trees should be diversified along Main Street. It is critically important that the right tree species be chosen for the planting area.

Street trees should be planted where planters are removed. This will remedy large gaps in shade particularly noted along the North side of Main Street between the Alley and Elm Street. Street trees should be tightly spaced where shade is most needed and spaced to provide meaningful shade at locations such as bus stops, curb extensions, outdoor cafes, etc.

Trees should be selectively replaced. Min. 2" caliper tree should be utilized. Branching height should be appropriate for a streetscape setting.

The tree species listed here have been selected to suit the climatic conditions of Oneonta, while providing a good shade canopy.

	Quercus bicolor Swamp White Oak		Japanese Zelkova <i>Zelkova serrata</i>		Tulip Tree <i>Liriodendron tulipifera</i>		Littleleaf Linden <i>Tilia cordata</i>
Form: Rounded	Growth Rate: Medium	Fall Color: Yellow	Environmental: Wet Sites and Drought Tolerant	Form: Vase-Like	Growth Rate: Medium	Fall Color: Red/Bronze	Environmental: Drought, pollution, high pH and high wind tolerant
Size: Large Tree (> 50')				Size: Large Tree (> 50')	Size: Large Tree (> 50')	Size: Large Tree (> 50')	Size: Med. Tree (35-55')



Ginkgo
Ginkgo biloba



**European
Hornbeam**
Carpinus betulus



Ornamental Pears
Pyrus spp.



Hawthorn
Crataegus spp.



Tree Lilac
Syringa Reticulata



Cherry
Prunus spp.

Form:
Upright

Form:
Upright

Form:
Rounded

Form:
Rounded

Form:
Rounded

Form:
Varies

Growth Rate:
Slow

Growth Rate:
Medium

Growth Rate:
Medium

Growth Rate:
Slow

Growth Rate:
Medium

Growth Rate:
Slow (generally)

Fall Color:
Yellow

Fall Color:
Yellow

Fall Color:
Maroon

Fall Color:
Maroon

Fall Color:
Yellow

Fall Color:
Red/Bronze/Yellow

Environmental:
Salt, Drought, High
wind, Pollution and
High pH tolerant

Environmental:
Salt, Drought, Pol-
lution and Shade
Tolerant

Environmental:
Salt, Drought, Pol-
lution and High pH
Tolerant

Environmental:
Salt and Drought
Tolerant

Environmental:
Salt, Drought and
Shade Tolerant

Environmental:
Varies

Size:
Med. Tree (35-55')

Size:
Med. Tree (35-55')

Size:
Intermediate Tree
(25-35')

Size:
Small Tree (< 25')

Size:
Small Tree (< 25')

Size:
Small Tree (< 25')

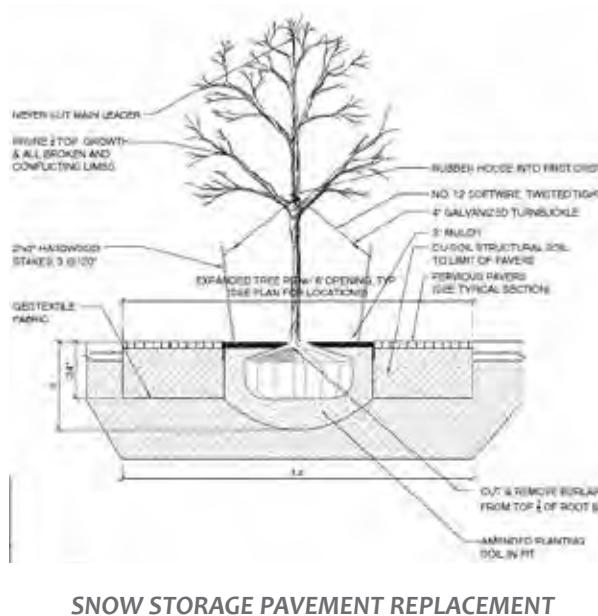
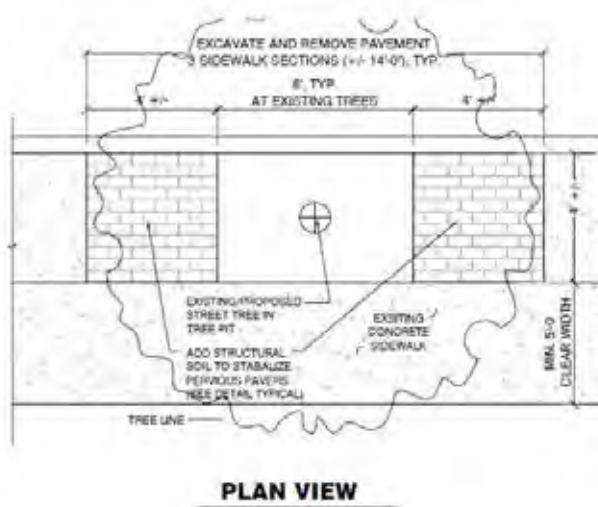
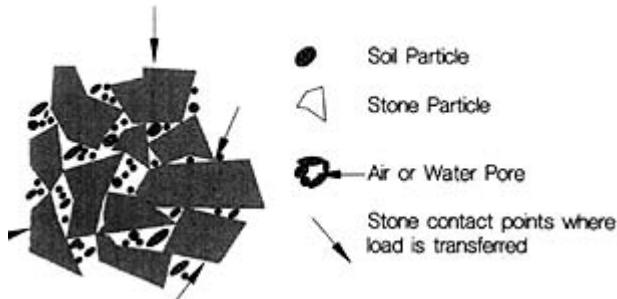
Structural Soils & Stormwater Management Considerations

Structural Soils

Healthy street trees need large volumes of soil with adequate water, nutrients and oxygen. Because soils are highly compacted in urban environments, this impedes root growth. Often trees die prematurely or older established trees cause sidewalk damage as roots grow beneath pavements.

Structural soils is a special medium designed to meet pavement design requirements while remaining root penetrable. It improves street tree vigor by providing a continuous medium for roots to grow freely and deeply without damaging the sidewalks. The expanded rooting zone better serves the long term needs of the tree.

With structural soils, the potential rooting zone of a street tree could extend from building face to curb, though this is not



usually feasible. Alternatively, a trench, running continuous and parallel to the curb can adequately serve the needs of street tree plantings.

Rooting zone can also be expanded by removing adjacent snow storage and/or sidewalk slabs and installing structural soils before replacing pavements. As an added benefit to the street tree, replacement pavements can be pervious.

Porous Pavement & Permeable Pavers

Porous pavements include pervious pavers, pervious asphalt or pervious concrete. Each material resembles it's conventional counterpart, but have more air spaces, which allow water to pass through the pavement into a 'open graded' base. The void spaces in the base (ie. crushed stone layers with no small or fine particles) stores and filters stormwater and reduces pollutants before allowing water to infiltrate back into the ground. Pervious pavement reduces stormwater runoff flow rate and volume.,

Pervious asphalt consists of coarse stone aggregate and asphalt binder, with very little fine aggregate. Water percolates through the small voids left in the finished



PERVIOUS PAVERS

asphalt. A thick layer of gravel underneath allows water to drain through quickly.

Pervious concrete consists of specially formulated mixtures of Portland cement, open-graded coarse aggregate, and water. It has enough void space to allow rapid percolation of water and resembles exposed aggregate concrete.

Permeable Pavers are comprised of a layer of pavers separated by joints filled with small stones. Pavers are gapped more generously than conventional paver installations, but nonetheless still meet ADA standards. The stones in the joints



PAVEMENT REPLACEMENT w/ STRUCTURAL SOILS & PERVIOUS PAVERS (*Montour Falls, New York*)
provide 100% surface permeability.

Main Street Opportunities

Opportunity exists along Main Street to expand the existing rooting zone of mature trees by selectively removing and replacing snow storage pavement. Pavement, pavement base and compacted sub-grade should be removed to proper planting depths. All excavation should be performed by hand or with air spading to avoid damage to feeder roots. Soil volumes should be replaced with a specified amended soil mix or structural soils below new pavement. To improve infiltration, pavement should be replaced with porous pavers.

Identity, Wayfinding & Information

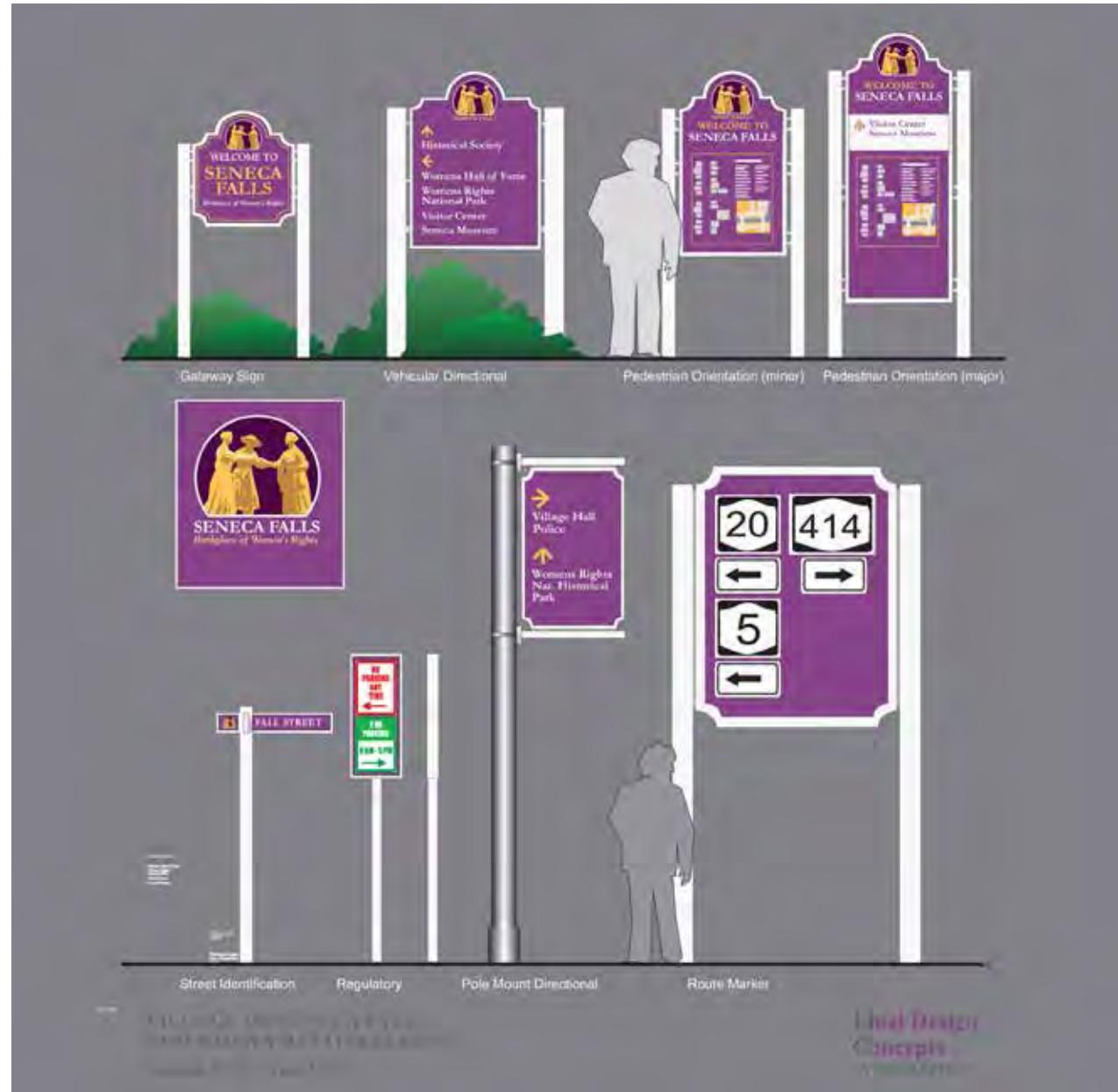
Architectural signage should be introduced to the street corridor to reinforce identity and create wayfinding for pedestrians and drivers. A family of signs should be designed to reinforce this sense of place and identity for Oneonta. A committee of municipal, commercial and private representatives should be appointed to work with a design professional to establish a design standard.

COMMUNITY BRANDING/IDENTIFIERS

The goal of any destination is to provide a UNIQUE sense of place. Community identifiers/branding accomplishes this. Their design inspiration can be drawn from many different sources: architectural elements, special cultural or historical connections, natural attractions, etc. These elements should be unique to each community and can be repeated throughout the design of a streetscape. Identifier elements are often found in signage and customized site amenities.

GATEWAY & AREA MARKERS

A gateway or area marker is generally a large sculptural element positioned at the end/beginning or along a streetscape. These features serve the purpose of marking the "entry" into an area of interest. This feature can be





repeated where appropriate to designate “downtown” Oneonta. Locations identified in this assessment include: Dietz Street, Chestnut & Main and Elm & Main.

Area markers can also be found in pavement as sidewalk medallions.

INFORMATION/KIOSK SIGNAGE

The purpose of informational signage is to convey information to the public about the commercial/retail destinations, area attractions, historical information and local events taking place. Both permanent and temporary information should be offered.

LIGHT POLE ENHANCEMENTS: BANNER AND POLE IDENTIFIERS

Banners were recently added to all the light poles along Main Street. These features provide annual color and are usually changed.

Pole identifiers are more permanent in nature, usually mounted to light poles. Both pole identifiers and banners can be individually customized for area business.



WAYFINDING SIGNAGE

Wayfinding signage is important both at the vehicular and pedestrian level. A comprehensive system of signs will promote a better visitor experience and improve traffic patterns by providing essential information that a visitor and resident needs to find and navigate to key downtown destination. A wayfinding sign system is based around the dynamics of the visitor experience as people approach Main Street, orient themselves at gateway points and find their destinations.

Signs should be used to identify destinations, direct vehicles and pedestrians and impart information. Signs should be consistent. Vehicular signage should be used to direct visitors to parking facilities and visitor destinations. Pedestrian-oriented directional signs can be attached to lampposts. Signs should feature visitor destinations. Signs should be located at starting/endpoint of walks. Information signs should be located at key points of arrival.

Design Concepts



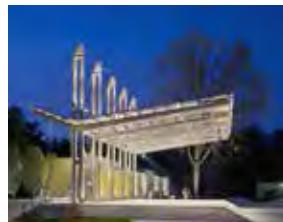
PLAZA SIGNAGE:

Muller Plaza is a key destination within the studied corridor . The plaza is programmed for many uses and events and offers varied seating to the public. An information kiosk could be added to the plaza to highlight upcoming events and orient the public with a map of area destinations.

BRANDING/IDENTIFIER SIGNAGE:

Existing amenities that will remain could be retrofitted to provide a character and unique sense of place within Main Street. All elements of the sign, form, color, font, logo and even support and landscaping all contribute to the "brand" that is being marketed. A wayfinding consultant should be employed on the design team to lead the committee through a logical process and to develop options for consideration.





Stage Examples (Left): Centerway Square (Corning, NY); Peacemakers Plaza (Binghamton, NY);

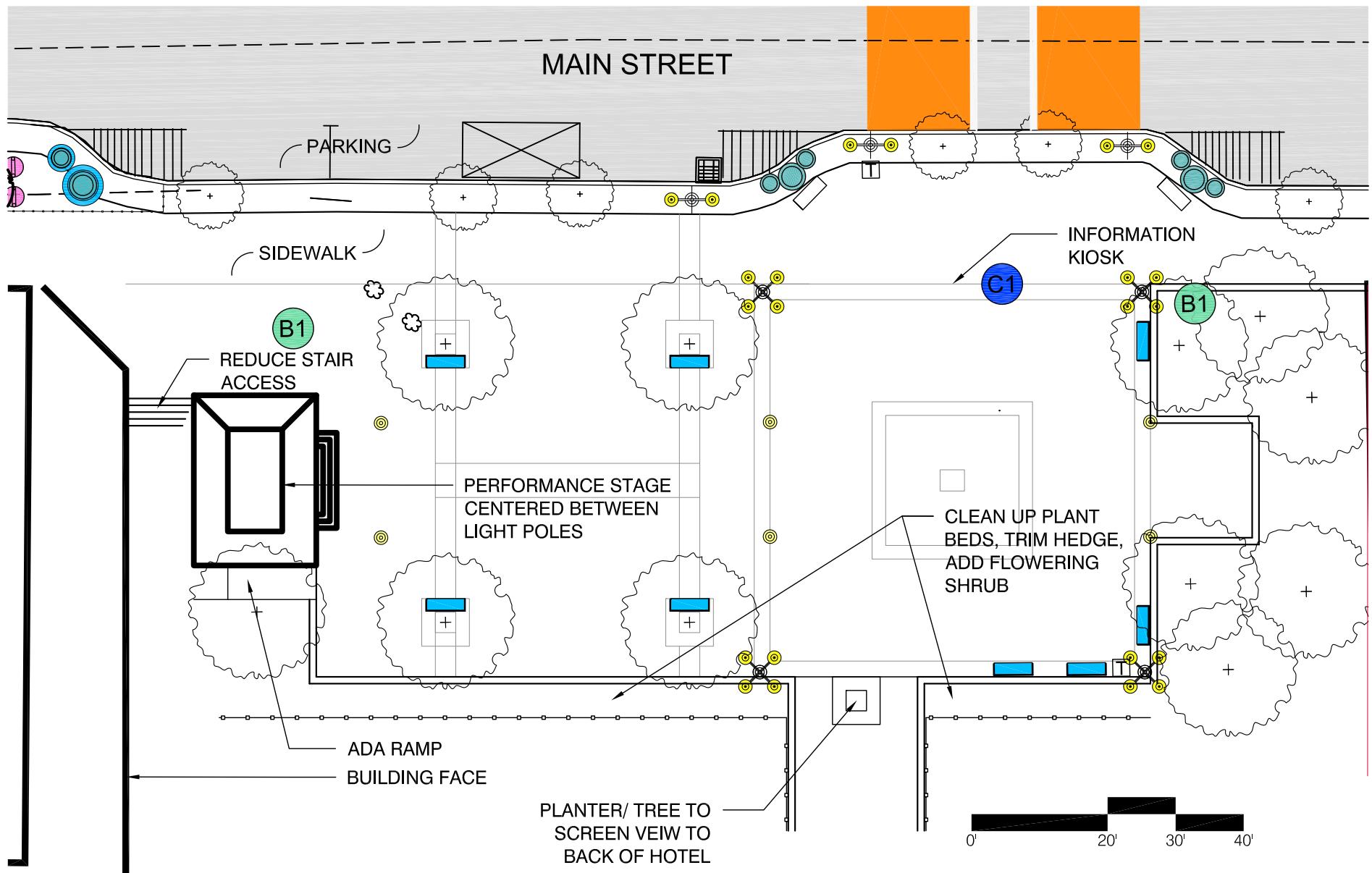
Bandstand Examples (Right): Festival Park (Fayetteville, North Carolina); Wavebeam Performance Shelter Example; ; Unionville Millennium Bandstand (Ontario, Canada); Memorial Bandstand (Waterville, NY); Market Stage (Winnipeg, Manitoba)

OUTDOOR PERFORMANCE

Aside from being an excellent source of revenue, outdoor performance spaces can be the focus of community and social activities. The design of a performance space should offer opportunities for special events, temporary performances, community gatherings, school performances, and everyday uses for individual food vendors and performing artists. People should be able to engage the space in many ways.

Site amenities and trees should be placed carefully to preserve long distance views from spectators but also provide much needed shade for comfort. The performance space should be pedestrian-friendly and handicapped accessible. Attention should be given to space for deliveries and staging. Adequate electrical power for lighting and sound is critical to the future success of the structure.

The design character of a stage structure could take many forms. For example, Main Street store fronts have awnings, lightpoles have steel supports and brick is a common material. The structure could have a historic connection to the Trolley or other feature.



PLAZA IMPROVEMENTS

Muller Plaza is the ideal location for a performance venue. The plaza is generally open in layout and provides ample space for small events. The memorial can be relocated in the stage floor and the plaque mounted to the railing. Stair access to the parking can be reduced by maintaining the section closest the building. Existing plantings can be added to and/or replaced to provide seasonal interest and aesthetic value.



LEGEND

	CONCRETE PLANTER
	BENCH
	LARGE STREET TREE
	SMALL-MEDIUM STREET TREE
	PROPOSED STREET TREE
	PROPOSED SHRUB/PERENNIAL
	LIGHT POLE (FOUR ARMS)
	LIGHT POLE (DOUBLE ARM)
	LIGHT POLE (SINGLE ARM)/PEDESTRIAN SIGNAL POLE
	TRASH RECEPTACLE
	RECYCLING RECEPTACLE
	NEWSPAPER STAND
	BIKE BOLLARD
	SIGN
	EXISTING SIGN



SIMULATION: Performance Stage

GATEWAY: Elm Street

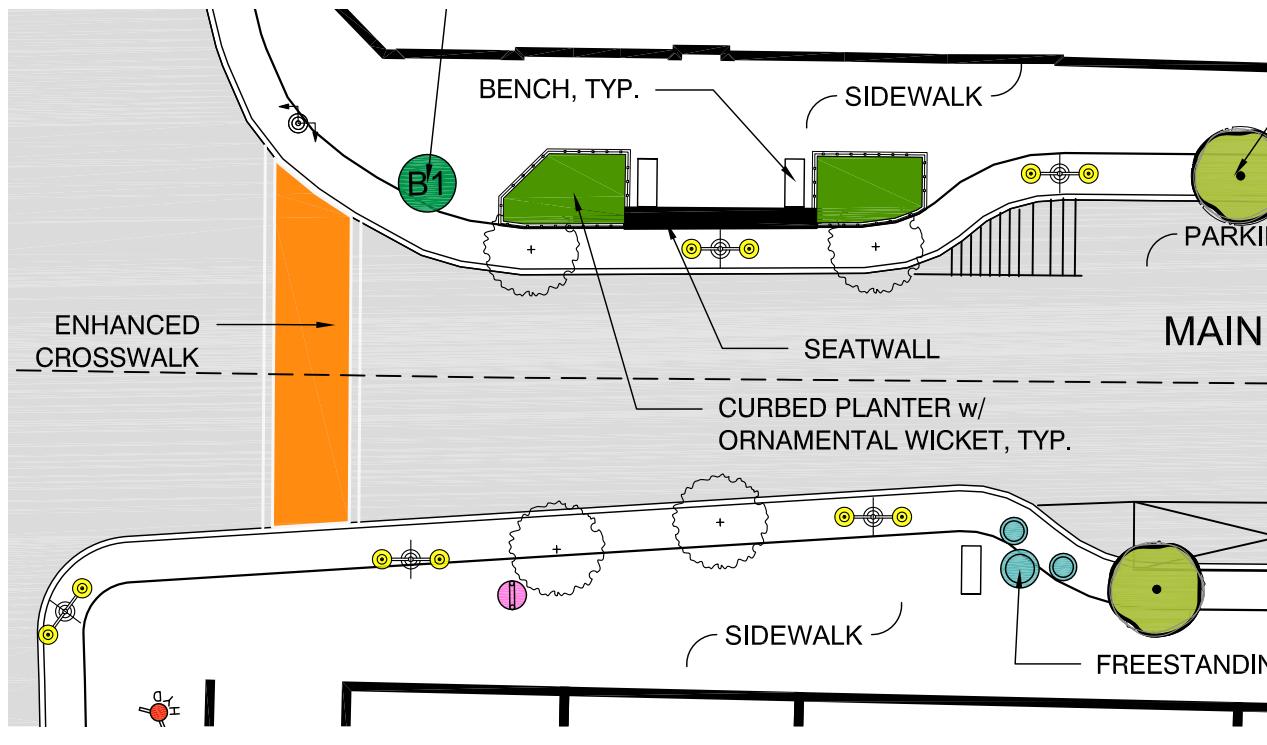
Elm Street can be enhanced to become a "gateway" to downtown with the addition of an identity marker, signage, bollards and ornamental wicket.





GATEWAY: CHESTNUT STREET

Chestnut Street can be enhanced to become a “gateway” to downtown with the addition of an identity marker. This marker can display welcome signage and also have pedestrian wayfinding signage facing the sidewalk. Because it is unclear where “downtown” actually begins, this element can be repeated at other appropriate locations outside the area studied in this assessment.

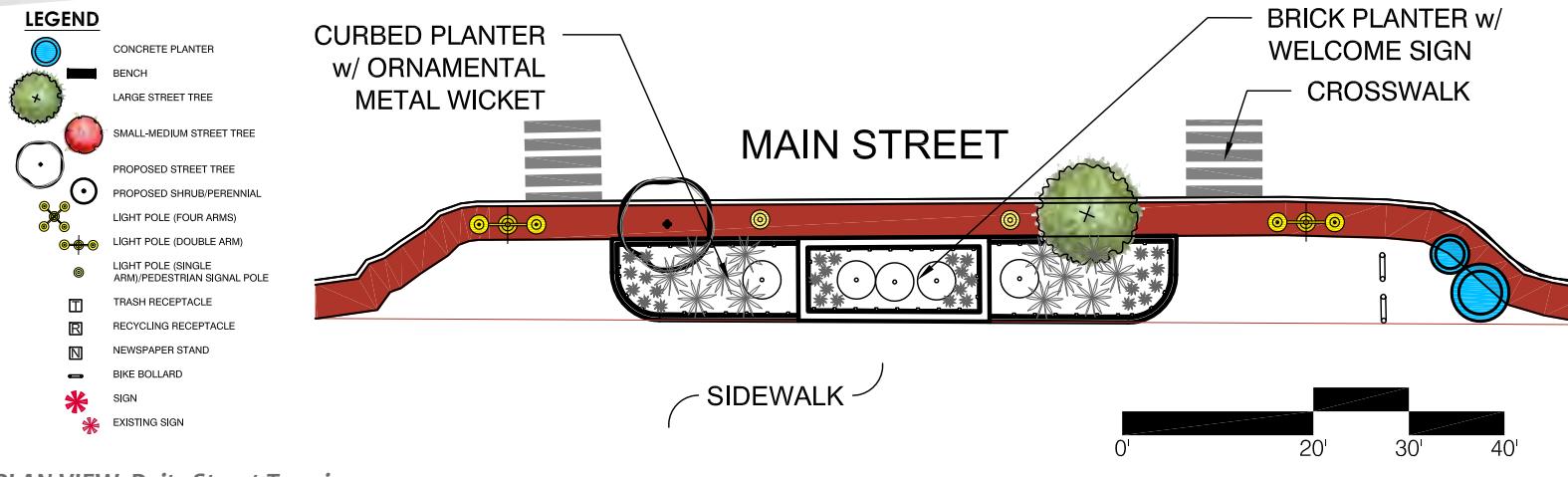


LEGEND

- CONCRETE PLANTER
- BENCH
- LARGE STREET TREE
- SMALL-MEDIUM STREET TREE
- PROPOSED STREET TREE
- PROPOSED SHRUB/PERENNIAL
- LIGHT POLE (FOUR ARMS)
- LIGHT POLE (DOUBLE ARM)
- LIGHT POLE (SINGLE ARM)/PEDESTRIAN SIGNAL POLE
- TRASH RECEPTACLE
- RECYCLING RECEPTACLE
- NEWSPAPER STAND
- BIKE BOLLARD
- SIGN
- EXISTING SIGN



SIMULATION: DEITZ STREET TERMINUS



GATEWAY:

DEITZ STREET TERMINUS

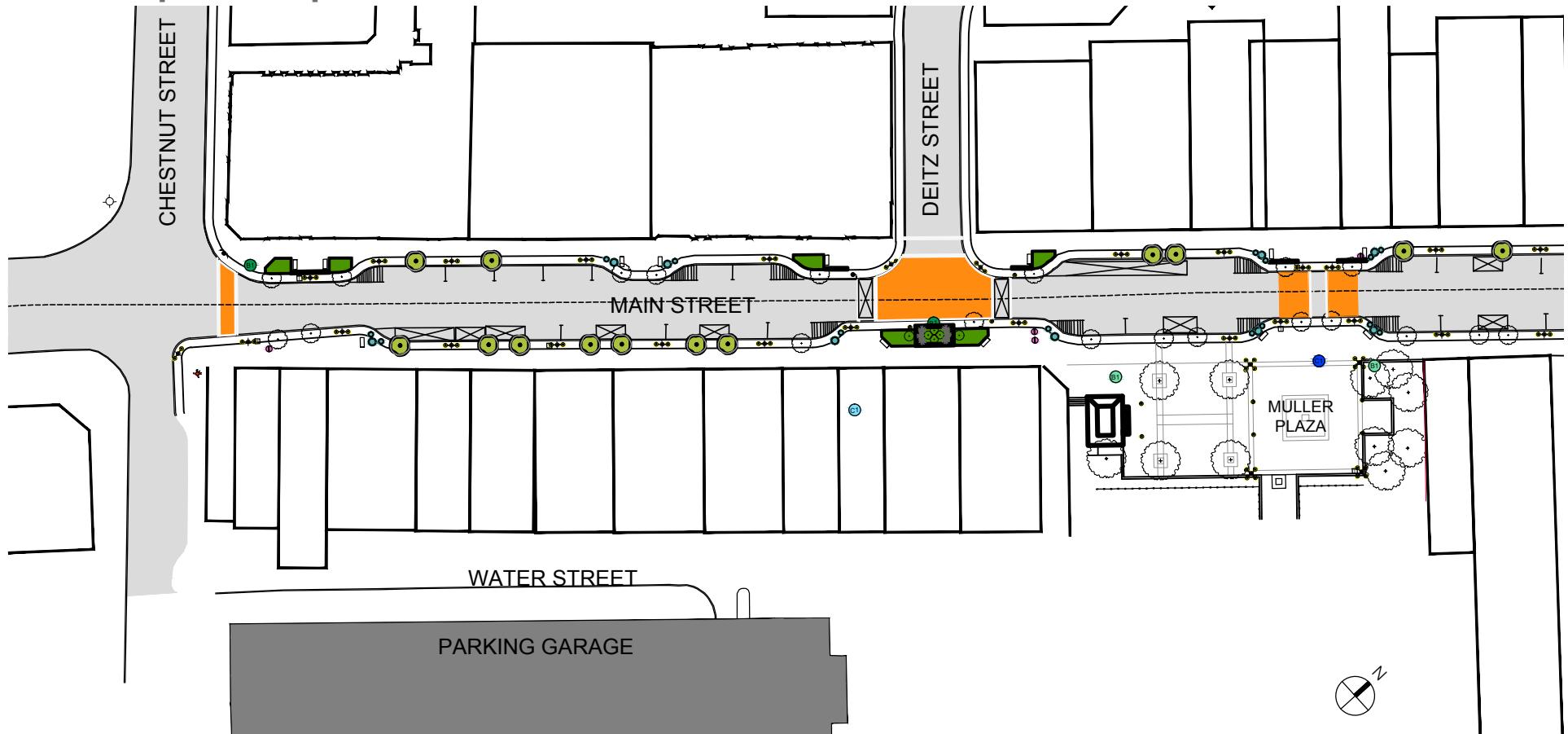
Deitz Street terminus can be enhanced to become one of Main Street's "gateways" to downtown. A raised brick planter has been simulated behind the curb, aligned with the centerline of the street. The planter can be planted with a few shrubs and perennials and supplemented with annual plantings. Curbed planters flank both sides and are shown planted with only shrubs and perennials.

All planters are shown simulated with an ornamental wicket. The wicket design was inspired from the existing fencing found along Main Street. Fencing will provide both a protective and decorative element to the planters. It can be customized to display a metal cutout, "Oneonta" and combined with the City's accepted slogan (the simulation utilizes the "city of the hills")



SIMULATION: Metal Wickets @ Plant Bed

Streetscape Concept Plan



EXISTING STREETSCAPE ELEMENTS

- Street Light (w/ 1 head)
- Street Light (w/ 2 heads)
- Street Light (w/ 4 heads)
- Traffic Light
- Crossing signal
- Existing Tree
- Existing Sign
- Empty/Infilled Tree Pit
- Planter
- Tree in Planter
- Bike Rack
- News/Mag. Rack
- Trash Receptacle

PROPOSED AMENITIES

- Recycling Receptacle
- Postal Box
- Bench
- Bus Stop
- Crosswalk
- Proposed Bench
- Proposed Bollard
- Proposed Freestanding Planter
- Proposed Seatwall

EXISTING UTILITIES

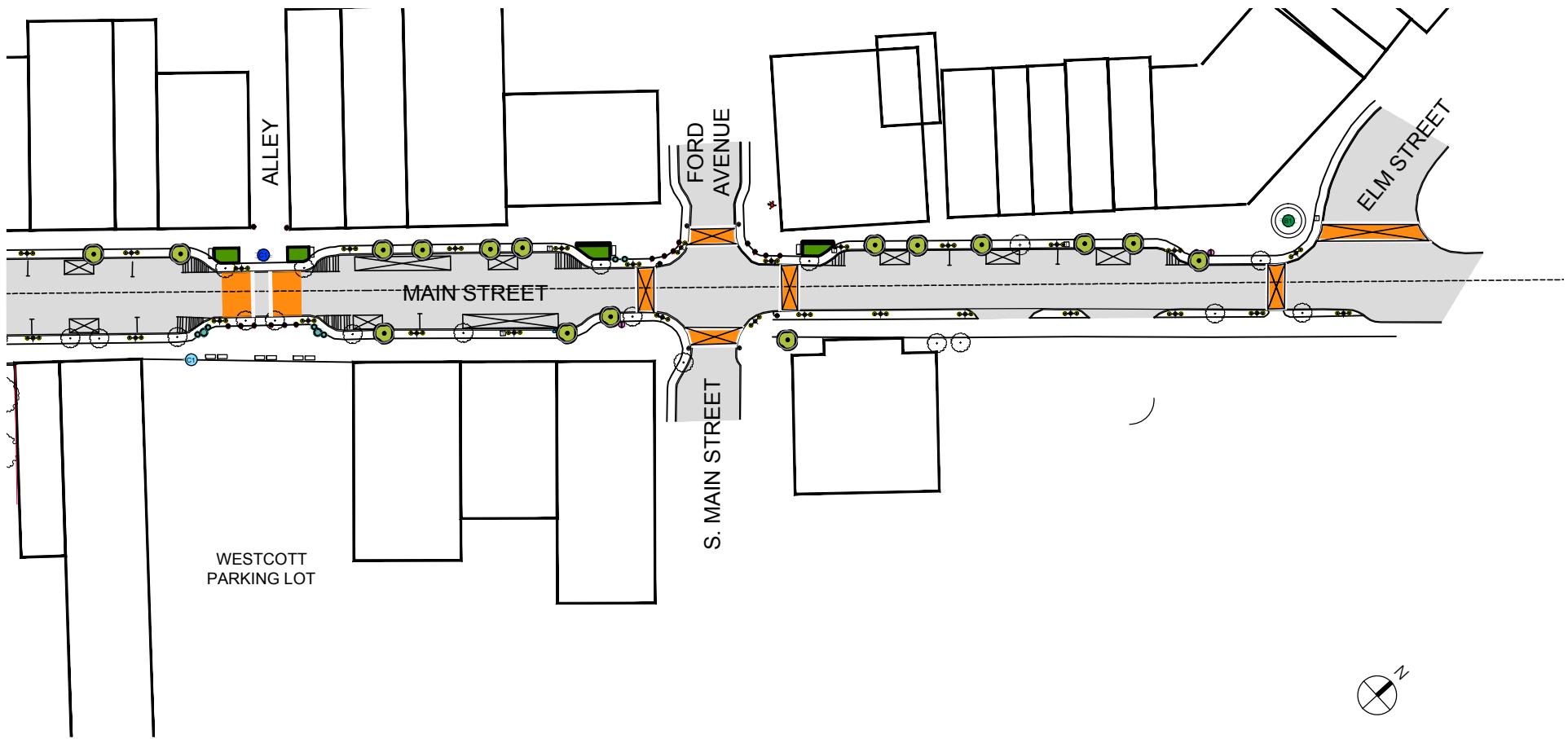


PROPOSED SIGNAGE

- A1 Community Identifier
- B1 Gateway/Area Marker
- B2 Directional (Vehicular)
- B3 Directional (Pedestrian)
- C1 Information/Kiosk
- C2 Historical
- C3 Other

PROPOSED GREEN SPACE/VEGETATION

- Proposed street tree
- Proposed shrub/perennial
- Proposed Planter (Flush, Curbed, Raised)



EXISTING STREETSCAPE ELEMENTS

- Street Light (w/ 1 head)
- Street Light (w/ 2 heads)
- Street Light (w/ 4 heads)
- Traffic Light
- Crossing signal
- Existing Tree
- Existing Sign
- Empty/Infilled Tree Pit
- Planter
- Tree in Planter
- Bike Rack
- News/Mag. Rack
- Trash Receptacle

PROPOSED AMENITIES

- Recycling Receptacle
- Postal Box
- Bench
- Bus Stop
- Crosswalk
- Proposed Bench
- Proposed Bollard
- Proposed Freestanding Planter
- Proposed Seatwall

EXISTING UTILITIES

- FIRE HYDRANT

PROPOSED SIGNAGE

- A1 Community Identifier
- B1 Gateway/Area Marker
- B2 Directional (Vehicular)
- B3 Directional (Pedestrian)
- C1 Information/Kiosk
- C2 Historical
- C3 Other

PROPOSED GREEN SPACE/VEGETATION

- Proposed street tree
- Proposed shrub/perennial
- Proposed Planter (Flush, Curbed, Raised)

Downtown Events - 2013

Date of Event	Time of Event	Event Name	Sponsoring Organization	Note
April 6, 2013	11:30 - 1:00 p.m.	Heart Association Walk	American Heart Association	Approved - No street closure required. Applicant to confer with OPD.
April 11, 2013	7:00 - 8:00 p.m.	Take Back the Night	Violence Intervention Program - OFO	Approved - No street closure required. Applicant to confer with OPD.
April 21, 2013	12:55 - 1:05 p.m.	SADD Run	Otsego Co. DWI Program	Approved - No street closure required. Applicant to confer with OPD.
April 27, 2013	1:00 - 5:00 p.m.	OH-Fest	SUNY Oneonta/Hartwick College	Common Council approved closure 11:00 a.m. - 6:00 p.m.: March 5, 2013
April 27, 2013	9:00 - 10:00 a.m.	Opening Day Parade	Oneonta Little League	Approved - No street closure required. Applicant to confer with OPD.
May 18, 2013	1:00 - 5:00 p.m.	Garage Sale	Townsquare Media	Approved after consultation with Mayor
May 19, 2013	9:00 a.m. - 10:00 a.m.	Riverside Elementary 5K	Riverside Elementary School	Approved - No street closure required. Applicant to confer with OPD.
May 28, 2013	10:00 - Noon	Memorial Day Parade	American Legion/City of Oneonta	Approved after review by Clerk and Police Chief. Street closure - same time as event.
July 10, 2013	6:00 - 7:00 p.m.	YMCA/Outlaw Run	Oneonta Family YMCA	Approved after review by Clerk and Police Chief. Street closure - same time as event.
September 6, 2013	4:00 - 9:00 p.m.	Fabulous Friday Cruise In	Main Street Oneonta	Approved after review by Clerk and Police Chief. Street closure - same time as event.
September 7, 2013	11:00 - Noon	American Legion Legacy Run	American Legion Riders	Approved - No street closure required. Applicant to confer with OPD.
September 14, 2013	9:00 a.m. - 4:00 p.m.	Grand and Glorious	Catskill Choral Society	Approved after review by Clerk and Police Chief. Street closure - 6 a.m. - 5:00 p.m.
September 14, 2013	9:00 - 5:00 p.m.	City of the Hilsi Arts Festival	UCCA (CANO)	Approved after consultation with Mayor for Dietz Street Lot
October 6, 2013	10:30 a.m. - 1:00 p.m.	Pitt Run	Ricky J. Parisian Scholarship Fund	No paperwork received yet.
October 31, 2013	7:00 - 9:00 p.m.	Halloween Parade	Lion's Club	No paperwork received yet.
No date set yet	2:00 - 2:30 p.m.	Tree of Lights	Catskill Area Hospice	No paperwork received yet.
No date set yet	9:30 - Noon	Holiday Parade	Main Street Oneonta	No paperwork received yet.
No date set yet	4:00 - 9:00 p.m.	Tree Lighting	Main Street Oneonta	No paperwork received yet.
December 31, 2013	1:00 - 2:30 p.m.	First Night Frostbite 5K	Oneonta Family YMCA	Approved after review by Clerk and Police Chief. Street closure - same time as event.

Downtown Oneonta is host to a number of community events.

Many business have expanded their storefronts to the sidewalk. Extended sidewalk space created by the bump-outs can be utilized for outdoor seating.

Maintenance-SAMPLE

WATERING: Water should contain a trace amount of fertilizer. Baskets in exposed areas should be watered at least 6 - 7 times per week whereas baskets in sheltered locations 4 - 5 times, depending on the weather. Ground planters should be sampled weekly for moisture content; appropriate corrective measures should be taken, if required.

TREE, SHRUB & PERENNIAL CARE: To be performed by trained professionals on a scheduled or on an "as needed" basis. Vegetation should be routinely cut back immediate adjacent to the sidewalk to prevent overgrown shrubs and low-hanging branches from obscuring signs and posing a hazard to users. Adequate clearance and sight distances should be maintained for safety. Dead-heading of perennials should be left to trained staff or professionals.

MULCHING: Besides improving the aesthetics of a planting, mulching plays an important role in vegetation establishment by preventing weed growth, conserving moisture in soil, stabilizing soil temperatures, minimizing erosion and improving soil characteristics by adding organic matter, etc. Mulch should be applied annually in the spring after soil

has warmed up and begun to dry out from winter. Spread mulch uniformly to a minimum 2 inch depth on the surface of the soil under the plants. Care should be taken to reapply mulch to all plantings for the first 3 years. When reapplying mulch over an existing base, bring the total depth up to 4 to 4.5 inches. Mulch type should preferably be shredded hemlock bark or hardwood.

WEED CONTROL: Weeds that create a hazard to users should be removed immediately; employ environmentally safe weed removal methods.

LEAF AND DEBRIS REMOVAL: Walkway surfaces should be kept clean. Falling leaves and other seed pods need to be removed regularly along with tree branches to ensure the safety of users. Wet leaves may become slippery and often leave stains on pavements that are difficult to remove. Inspections for potentially hazardous trees or limbs should also be made regularly.

TRASH REMOVAL: Litter should be collected and trash receptacles emptied regularly. Especially following a major event or promotion.

AMENITIES: Maintenance of street-scape amenities (i.e. benches, light poles, bike racks, interpretive signage, trash bins, etc.) should be inspected regularly. Missing or damaged signs should be promptly replaced. Save warranty records on manufacture products when received. Specifications should call for touch-up paints and replacement parts.

SIDEWALK REPAIR: If the needed repair to sidewalk surfaces is deemed a hazard, repairs should be made immediately. Users should be directed away from areas under repair with temporary signage; if this is not possible, temporary sidewalk closings should be made.

VANDALISM: Any vandalism should be removed promptly so as not to encourage re-occurrences. Vandals who are prosecuted should be encouraged by court order to attend community service events that are aimed at community maintenance.

GRAFFITI: Improvements including raised planters and other surfaces may encourage graffiti tags. The City must have a mechanism for swift control and elimination of these unsightly blemishes.

IMPLEMENTATION STRATEGIES

Strategies for Implementation

Numerous strategies for realizing the streetscape improvements for Main Street are offered for consideration depending on the level of Federal, State and Local funding available and level of organization and financial support the City is capable of committing over a multi-year schedule. This document should provide a major step toward securing financial support for many improvements. Nonetheless, major improvements to Main Street paving infrastructure are more probable with Federal and State grant monies picking up the bulk of the costs. These improvements can be supplemented with in-kind services by City departments, and financial support from other Civic groups.

This said, the current financial picture for New York State is very unclear with new leadership attacking high deficits and cost-cutting measures more the priority for the foreseeable future. Indications are that the major funding sources will remain intact with small to moderate reductions in annual allocations. A look at recent projects around the state with similar components to Main Street shows an average allocation of \$50,000 to \$1,500,000 to municipalities, for similar projects. A number of potential funding

sources are described later. State and/or federal grant monies provide the impetus for new development that might never be accomplished with local funding only. Usually requiring matching funds, these grants place more responsibility and ownership on the applicant, thus helping to assure the project's success. However, there are associated costs with public money of which the applicant must be aware. These come in the form of administration of contracts and requirements for tighter specifications, bidding procedures, bonding and construction review services.

When federal or state grant monies are involved in a project, the lines between contracted services, donated services, and in-kind services must be clearly defined. Many grant programs allow the value of in-kind services provided by the municipality to be used toward the required match. These need to be well documented. To construct the major street improvements identified in this study, grant monies will be necessary from outside sources. Federal sources usually limit in-kind support.

In-kind services may be used to imple-

ment a range of smaller improvements. City in-kind could also come from Planning efforts, engineering, parks, or public works. Discussions with the City Department of Public Works have indicated that the City could be responsible for general demolition and/or removal of pavements, light pole upgrades, site amenities and trees. Precedent also exists for private community and civic groups to donate funds toward the construction and revitalization of facilities. A service club might be approached to sponsor construction of the stage or purchase of basket planters. Other businesses or clubs could donate amenities such as benches, signs, and plantings.

In all phases, construction drawings and estimates will need to be prepared for the proposed improvements. This document and the Schematic Plan drawings are not considered "construction documents". Costs for the detailed construction drawings and specifications are accounted for in the Schematic Estimate found in this chapter, as they are necessary to bring this or any other project from the Schematic phase to actual implementation. Survey updates could be completed by Engineering staff in phase 1 of planning.

The following strategies are offered for consideration depending on the level of funding that is approved and the support provided by the community:

- Continue reaching out for local and legislative support of the Main Street Streetscape improvement projects. Public outreach will be necessary early in the process. It must continue with an organized, balanced Advisory Committee and local (state) support.
- Lobby for legislative support of current applications and/or improvements. Invite attendance at civic events, 4th of July Celebration, etc. Be persistent!
- Secure property survey and easements or other means of negotiated passage for private properties within the corridor.
- Update the utility survey for design development and construction either with in-kind or contracted services.
- Assure in-kind services and/or budgeted costs for anticipated funding

(match) is accurate and approved by the City Council.

- Utilize in-kind services and donations of labor and materials to construct certain improvements and weigh the impact of these donations to the public funding match. Be sure to contact the grantor for verification of match eligibility.
- Pursue technical assistance grants for specific projects, which can often be included as part of your match to larger funding sources. (i.e.; New York State Urban and Community Forestry Council Community Improvement Grants, Municipal Challenge Grants, CDBG, DOS, DEC, LWCF and ARC support). NY Main Street Grants often have a small setaside for streetscape improvements.
- Include in City (Parks and Public Works) budget fixed annual commitments toward improvements and maintenance. Continue budgeting for capital improvements in subsequent years.

- Solicit area Civic organization and local foundations to consider funding streetscape and/or stage elements.

See potential Funding Sources in the end section of this report.

Implementation Priorities

The following table of implementation priority projects have been assembled using the Design Recommendations described in earlier sections of this report. These recommendations were reviewed by the MSO design committee, the Council's Community Improvement Committee (CIC), the Mayor and the City Engineering department. A second table of priorities was assembled from their comments and follows the Implementation Recommendations.

It is suggested that these priorities be reviewed annually and reorganized based on funding, public sentiment and City initiatives.

Implementation Recommendations

Category	Task	Project Type & Description	Lead Project Partners/ Funding Sources	Time Frame
HIGH IMPACT/LOW BUDGET	A	Upgrade Pole Fixtures Replace existing light pole fixtures with LED's (already purchased by Village)	City of Oneonta DPW	Current retrofit installation 2013-14
	B	Street Tree Pruning & Removals Selectively prune existing trees within corridor. Remove trees in raised planters.	City of Oneonta DPW Prof. Design Consultant	Tree trimming and removals continue to be annually phased over late fall and early summer seasons.
	C	Street Re-striping Assess existing parking/no parking and bus stop locations. Re-stripe corridor to maximize parking. See plan for recommendations.	City of Oneonta DPW	Annual maintenance
	D	Appoint Steering Committee Appoint a committee of appropriate stakeholders to guide and finalize design decisions. A good cross-section of business owners, artists, and public officials should be represented to establish the vernacular for Main Street.	City of Oneonta	Immediate buy-in of stakeholders is important. Fill 1 year terms while momentum remains with project.
	E	Paint or Repair Existing Metal Railings Identify sections in need of repair and schedule painting.	City of Oneonta DPW/Parks	Summer temporary help could be engaged to paint.
	F	Restore Flower Baskets on Light Poles Seek sponsor for hanging baskets on light poles. Most growers start cuttings in February for May installations. Baskets will need to be purchased and brackets assessed for durability.	City of Oneonta Steering Committee	Secure sponsor during early winter months to allow time for baskets to be purchased and plantings to be grown.
	G	Traffic Bollards Install traffic bollards stored by DPW at appropriate locations. Seek final design of streetscape improvements prior to installing.	Landscape Architect City of Oneonta DPW	Possible mid-range project as these will be tied to other major improvements
	H	Raised Planters Upgrade existing raised planters to remain with decorative enhancements (brick, mosaic, etc.). New freestanding planters could be purchased to replace old concrete units.	City of Oneonta DPW Steering Committee Local Artist	Upgrade if planters are to be salvaged. Streetscape design to locate new.
QUICK WIN PROJECTS	I	Benches Repair or replace existing benches in disrepair. Give consideration to bench length and seating options. Look to replicate existing styles.	City of Oneonta Donor Program/recognition	Budget for 2014 repairs and replacement slats to existing.
	J	Secure Efficient Watering System for Maintaining Seasonal Plantings Purchase a 300-500 gallon truck mounted system with pump and fertilization component. Budget staff to supplement watering of planters, baskets and new trees.	City of Oneonta Parks	Purchase truck mounted unit for Spring 2014.
	K	Street Tree Recommendations & Species Criteria Identify selection of new street tree locations and species. Establish varieties and minimum sizes.		

CATEGORY	TASK	PROJECT TYPE & DESCRIPTION	LEAD PROJECT PARTNERS/ FUNDING SOURCES	TIME FRAME
QUICK WIN PROJECTS	L	Update Amenities Establish acceptable list of amenities and honor it. Finish updating trash and recycle receptacles. Limited freestanding planter pots can be purchased for spring installation.	Landscape Architect Steering Committee City of Oneonta DPW	Annual budget allocations over 2-3 year period, or with project funding
	M	Traffic Signal Calibrations Calibrate signal sequence to accommodate pedestrians.	City of Oneonta DPW	Primary issue noted at Chestnut St intersection.
MEDIUM-TERM IMPROVEMENTS	N	Street Trees in Vacant Tree Pits Install street trees at vacant tree pit locations.	City of Oneonta Parks	Apply for DEC Urban Forestry Grant (Dec. 2013)
	O	Street Tree Replacement Remove and replace Norway Maples near Chestnut Street intersection. Identify existing utilities and subsurface conditions.	City of Oneonta DPW Prof. Design Consultant	Tree trimming and removals continue to be annually phased over late fall and early summer seasons.
	P	Tree Pit Expansion/Flower Bed Upgrades & Removals Identify locations where root flare has heaved pavement and excessive pavement is unnecessary. Selectively remove pavement to expand plant bed. Excavate to required depths for improvements. Install new curb, protective fencing and perennial plantings.	Landscape Architect City of Oneonta DPW General Contractor	Project is related to raised planters and requires design development to retrofit to areas of streetscape.
	Q	Legal Survey Secure property survey and easements or other means of negotiated passage for private properties the targeted area. Locate existing utilities, sidewalk vaults, curb lines, vegetation and site amenities. Establish benchmarks in field.	DPW Staff or NYS Professional Surveyor	Immediate need to have accurate data to work from. City forces may supplement existing data, or hire prof.
	R	Tree Pit Wickets Identify locations where existing tree pits will remain. Steel wickets can be fabricated to protect base of trees from compaction and hold mulch in place. Plantings can also be maintained to add color and life to Main Street.	Landscape Architect Steering Committee Metal Fabricator City of Oneonta DPW	Fabrication of test sample is recommended. Larger order to be tied to funding and other ornamental work.
	S	Farmers Market & Stage @ Muller Plaza Design structure to fit into context of plaza and handle capacity of planned performers and events. Allow for adequate power and potential to add portable lighting system.	Landscape Architect City of Oneonta DPW General Contractor	A must to add activity and life to a downtown. The Plaza is ideal for this amenity. Funding can come from a variety of sources.
	T	Design and Retrofit Existing Amenities with Branding/Identifier Signage	City of Oneonta DPW Wayfinding Consultant	Examples were presented with concepts for planters, bike bollards, and railings.

Category	Task	Project Type & Description	Lead Project Partners/ Funding Sources	Time Frame
Landmark/Long Term Projects	U	Raised Brick Planters Selectively remove pavement and existing raised planters, rearrange site amenities and install expanded raised planters. Material selection to be durable and historically relevant.	Landscape Architect City of Oneonta DPW General Contractor	A mid-range improvement due to the need for survey and detailed design. A pilot project could be catalyst for remainder of downtown.
	V	Sidewalk Reconstruction Upgrade snow storage and sidewalks where outdated or in disrepair. Replace old pavers with new on sustainable base. Paver type and color selection is important to reinforce streetscape character. May be implemented a block or side at a time.	Landscape Architect/PE City of Oneonta DPW Business District / Chamber	Another mid-range improvement due to the need for detailed design. Could be bundled with planter/tree pit projects to be efficient.
	W	Midblock Crossings Install traffic calming (raised) or highly visible crosswalks at midblock locations such as Mueller Plaza.	Landscape Architect/PE City of Oneonta DPW	This should be coordinated with the addition of the Stage structure and can be phased in as needed.
	X	Family of Signs Select a consultant team to work with stakeholders/steering committee to design a family of signs and sign standard for Oneonta. This is a process that requires qualified professional	Wayfinding Sign Consultant Steering Committee City of Oneonta	Depending on funding, this can occur during first year of streetscape planning.
	Y	Street Intersection Reconstruction Upgrade traffic signals and pedestrian poles to meet current standards. Make all crosswalks and curb ramps ADA compliant. Enhance select intersections with decorative pavement treatment.	Landscape Architect/PE Traffic Engineer City of Oneonta DPW NYSDOT Region 9	Tied in with the bigger project of reconstructing Main Street and sidewalks. Some poles can be retrofit to existing infrastructure.
	Z	Gateway Feature at Exit 14 Ramp A branding opportunity exists to capture the commuting public attention within the ROW at the Exit ramp for I86 East. A permanent masonry or breakaway sign feature will require NYSDOT approval depending upon the location. Size matters here to be visible.	Wayfinding Sign Consultant Landscape Architect NYSDOT Region 9 General or Sign Contractor	The sign feature must meet MUTCD standards and incorporate colors, text and image specific to Oneonta.
	AA	Special Needs Pedestrian Enhancements These include curb articulations, pedestrian traffic signals, color contrasting pavements, bollards with chains, and other features currently required by ADA.	City of Oneonta DPW NYSDOT Region 9 Independence Center or Association for Visually Impaired	These improvements can be phased in with other work or may require more design development to coordinate improvements throughout.
	BB	Storm and Infrastructure Improvements Install stormwater, Sanitary, Water and other (buried) utility upgrades based on age and scheduled maintenance. Coordination with utilities will drive the schedule and process.	City of Oneonta DPW NYSDOT Region 9 NYSEG DEC	Coordination with utilities will drive the schedule. Early communication with liaison is recommended. DPW will have best insights.

Implementation Priorities (Prepared by MSO Committee)

1. CURRENT/ONGOING PROJECTS			
Project	Status/Action	Cost Notes/Options	MSO Grade Level
1a. LIGHT FIXTURE & POLE REPLACEMENT	Research options and design selections with light fixture rep; consider pole banner extensions; MSO supports the new Main Street poles and fixtures on Chestnut and on the west end of Main. How far should the new lighting extend? How far can the City afford to extend it? Should Market be included at some point?	Large Impact; seek funding; estimate cost in phases; implement CIP; Chestnut Street = \$150,000 budget	NA
1b. TREE PRUNING & REMOVALS	Pursue recommendations from specialist	In current operating budget; \$600?	NA
1c. PAVEMENT STRIPING	Pursue recommendations from specialist	In current operating budget; \$5,000	NA
1d. PAINTING RAILINGS	Ongoing; Set up maintenance schedule; shop materials	Low/no cost using volunteer labor	NA
1e. PAVING PARKING SPACES	Project slated for 2014; Inspect curbs	Current Budget = \$30,000	NA
2. SHORT TERM, SMALL SCOPE PROJECTS			
Project	Status/Action	Cost Notes/Options	MSO Grade Level
2a. BENCH REPAIR and/or REPLACEMENT	Received replacement estimate; Shop for replacement slats; Other lengths and backless available	\$1,900 ea.	A
2b. WATERING EQUIPMENT	A motorized water delivery vehicle is desired for more efficient and thorough watering practices	Move forward on developing inexpensive ideas such as a used or shared gator	B
2c. TREE EVALUATION & UPGRADES	Research and present options with a consultant		C
2d. TRASH RECEPTACLES	Research and present options; low priority; hope that Recyclables bins are also being considered.	\$1,350 ea.	D
3. INTERMEDIATE SCOPE PROJECTS			
Project	Status/Action	Cost Notes/Options	MSO Grade Level
3a. TREE PLANTINGS UPGRADES, REMOVALS	Research and present options, but start with trees that need to be replaced.	\$1,200 ea.	A
3b. FLOWER BED UPGRADES, REMOVALS	Keep or phase out? Pursue recommendations including maintenance; start with the intersection of Dietz & Main. B/C grade	\$1,000 ea.	B/C

3c. SURVEY & DATA COLLECTION	Obtain existing data first; verify existing plumbing, electrical circuits, draining type and locations	\$ 7,500±	A
3d. TREE WICKETS	MSO does not support; think they would be damaged and would be in the way of gardeners.	\$ 175 ea.	D
3e. MULLER PLAZA STAGE	MSO believes that private funding may well be available	\$?	A
3f. BIKE RACKS	Don't see the need for additional racks at this time	\$ 750 ea.	D

4. LONG TERM, LARGE SCOPE PROJECTS

Project	Status/Action	Cost Notes/Options	MSO Grade Level
4a. PLANTERS	Do in Phases; is design consultant req'd.? MSO comments: (Agrees that these should be done in phases. Should they all be removed? Why keep most on north side of the street, where there's too much shade? Could they be covered with brick or tiles? Think the planters should be at the level they are now.)	Detailed cost estimates on options and phases are needed	A
4b. PAVERS & CROSSWALKS	Do in Phases; design consultant req'd.; replace where planters or trees have been removed. "A" grade for a crosswalk at Muller Plaza; "C" grade for other crosswalks	Detailed cost estimates on options and phases are needed	A/C
4c. SINAGE	Knowledgeable professional and committee req'd.; MSO sees as an important step and would like to be a part of it	\$ Detailed estimates needed \$	B
4d. INTERSECTIONS REDESIGN	Design consultant req'd.	\$ Detailed estimates needed \$	C
4e. EXIT 14	Design consultant req'd.	\$ Detailed estimates needed \$	NA
4f. SPECIAL NEEDS ENHANCEMENTS/ BOLLARDS	Include with other projects as needs arise	\$ Detailed estimates needed \$	C

Cost Estimate

MATERIAL DESCRIPTION/SIZE	UNIT	QTY	UNIT COST
DEMOLITION			
EXCAVATION FOR PAVEMENTS	SF		1.45
UNCLASSIFIED EXCAVATION AND DISPOSAL	CY		35.00
STRUCTURAL EXCAVATION	CY		60.00
AIR SPADING EXCAVATION	CY		32.67
CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF		25.00
SAW CUTTING PAVEMENT	LF		3.00
BASIC WORK ZONE TRAFFIC CONTROL	HR		
REMOVE AND STORE LAMPPOST ASSEMBLY	EA		600.00
CROSSWALKS			
CURB RAMP - CONFIGURATION TYPE 1 - ADA COMPLIANT	EA		2,000.00
EMBEDDED DETECTABLE WARNING UNITS	SF		200.00
IMPRINTING AND COLORING OF ASPHALT CONCRETE PAVEMENT	SF		16.00
WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF		0.30
PAVEMENTS			
CONCRETE SIDEWALKS AND DRIVEWAYS	CY		500.00
COLORED AND IMPRINTED PORTLAND CEMENT CONCRETE SIDEWALK	CY		700.00
BLUESTONE UNIT PAVERS ON A PREPARED SETTING BED	SY		332.67
BRICK PAVED SIDEWALK AND DRIVEWAYS (SAND SETTING BED) (@ SNOW STORAGE)	SY		302.67
SELECT GRANULAR FILL	CY		48.00
CRUSHED STONE (IN-PLACE MEASURE)	CY		30.00
GEOTEXTILE SEPARATION	SY		2.45
STRUCTURAL SOILS	CY		48.00

MATERIAL DESCRIPTION/SIZE	UNIT	QTY	UNIT COST
PAVEMENT MARKINGS			
WHITE PAINT REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF		0.30
WHITE PAINT REFLECTORIZD PAVEMENT SYMBOLES - 20 MILS	EA		65.00
YELLOW EPOXY REFLECTORIZD PAVEMENT STRIPES - 20 MILS	LF		6.00
PLANTERS			
FREESTANDING PLANTERS (Type 1-24" dia.)	EA		745.00
FREESTANDING PLANTERS (Type 2-48" dia.)	EA		1,075.00
FREESTANDING PLANTERS (Type 1-72" dia.)	EA		1,275.00
STONE CURB, GRANITE, (TYPE A)	LF		30.00
CAST-IN-PLACE CONCRETE CURB	LF		38.00
16" RAISED PLANTERS w/ BRICK FACING & STONE COPING	LF		950.20
LIGHTING			
POLE EXCAVATION AND CONCRETE FOUDNATION	CY		2,500.00
PEDESTRIAN PUSHBUTTON AND SIGN WITH POST	EA		350.00
RELOCATE EXISTING STREET LIGHT ASSEMBLY	EA		815.83

MATERIAL DESCRIPTION/SIZE	UNIT	QTY	UNIT COST
SIGNAGE			
GATEWAY SIGN	EA		5,500
PRIMARY ORIENTATION KIOSK	EA		8,500.00
PRIMARY ORIENTATION KIOSK	EA		8,500.00
VEHICULAR DIRECTIONAL	EA		3,780.00
PEDESTRIAN DIRECTIONAL (LARGE)- EXISTING POLE MOUNT	EA		2,450.00
PEDESTRIAN DIRECTIONAL (SMALL)- EXISTING POLE MOUNT	EA		2,260.00
VEHICULAR TRAIL BLAZER - EXISTING POLE MOUNT	EA		480.00
FREE-STANDING INTERPRETIVE SIGN	EA		2,840.00
RAIL MOUNTED INTERPRETIVE SIGN	EA		1,600.00
HAZARD/WARNING SIGN	EA		1,430.00
SITE AMENITIES			
ORNAMENTAL BOLLARD (NOT INCLUDING MATERIALS)	EA	1	305.31
ORNAMENTAL WICKETS	EA	1	175.15
BIKE RACK	EA	1	745.11
BENCH, TYPE 01 (6' LENGTH)	EA		1,824.50
BENCH, TYPE 02 (4' LENGTH)	EA		1,625.00
BENCH, TYPE 03 (6' LENGTH & BACKLESS)	EA		1,700.00
TRASH RECEPTACLE	EA		1,344.00
4' SQ. TREE GRATE w/ FRAME	EA		1,400.00
4' x 6' TREE GRATE w/ FRAME	EA		1,600.00

MATERIAL DESCRIPTION/SIZE	UNIT	QTY	UNIT COST
PLANTING			
Planting Major Deciduous Tree Species (Large Street Tree)	EA		650.00
Planting Major Deciduous Tree Species (Medium Street Tree)	EA		550.00
Planting Major Deciduous Tree Species (Medium Street Tree)	EA		300.00
Planting Major Deciduous Tree Species	EA		330.00
Planting Major Deciduous Tree Species	EA		269.00
Planting Deciduous Shurb Species	EA		43.20
Planting Deciduous Shurb Species	EA		39.90
Planting Evergreen Shurb Species	EA		34.90
Planting Special Plant Materials	EA		22.05
Planting Special Plant Materials	EA		22.05
Planting Special Plant Materials	EA		22.05
Planting Vines & Ground Species	EA		5.90
Planting Special Plant Materials	EA		1.50
PLANT BED PREPARATIONS			
MULCHING	CY		85.00
PLACING TOPSOIL- TYPE A	CY		75.00
TOPSOIL, URBAN PLANTING MIX	CY		85.00
ESTABLISHING TURF	SY		3.50

Funding Sources: State Administered Grants

CONSOLIDATED FUNDING APPLICATION

Through the New York State Consolidated Funding Application (CFA), a single application for multiple sources of state funding is available for the following projects:

- Direct Assistance to Businesses and Other Organizations
- Community Development
- Waterfront Revitalization
- Energy
- Environmental Improvements
- Sustainability Planning and Implementation
- Education/Workforce Development
- Low Cost Financing

Application materials and details for workshops are available on line at www.regionalcouncils.ny.gov.

The funding solicitation opened on Monday, **June 17, 2013** and applications were accepted through the CFA until 4:00 pm on Monday, **August 12, 2013**.

Please be advised that all CFA Grants are subject to the New York State Executive Law Article 15-A which requires, where applicable, the establishment of minority-and woman-owned business

enterprises ("M/WBE") contract goals. For more information with regard to the M/WBE requirements, please contact the appropriate Regional Council for your project area for assistance.

Oneonta is part of the Mohawk Valley Regional Council: CFARegion6@ny.gov

Among the grants available through the CFA are:

1. NEW YORK MAIN STREET

Funding Available: Up to \$2.2 million

2. NEW YORK MAIN STREET TECHNICAL ASSISTANCE

Funding Available: Up to \$200,000

Description:

The New York Main Street (NYMS) Program provides resources to invest in projects that provide economic development and housing opportunities in downtown, mixed-use commercial districts. A primary goal of the program is to stimulate reinvestment and leverage additional funds to establish and sustain downtown and neighborhood revitalization efforts.

Eligible Types of Applicants:

Units of Local Government or organizations incorporated under the NYS Not-for-Profit Corporation Law that have been providing relevant service to the community for at least one year prior to application.

New York Main Street (NYMS)

Program Eligible Activities:

Applicants may request a total of between \$50,000 and \$200,000 for the following activities:

Building Renovation:

Matching grants available for renovation of mixed-use buildings.

Streetscape Enhancement:

In addition to building renovation funds, applicants may request up to \$15,000 in grant funds for streetscape enhancement activities, such as: planting trees, installing street furniture and trash cans, and performing other supportive activities to enhance the NYMS target area.

A streetscape enhancement grant will only be awarded as an activity ancillary to a building renovation project and cannot be applied for on its own.

Administration:

Applicants may request up to 7.5% of the grant amount for salaries and other costs associated with the administration of the grant. The administrative funds must be requested at the time of application, and must be included within the maximum request amount.

New York Main Street Technical Assistance (NYMS-TA)**Eligible Activities:**

Applicants may request up to \$20,000 for a New York Main Street Technical Assistance project. A minimum of 5% cash match is required. NYMS-TA projects must directly improve an organization and/or community's capacity or readiness to administer a future NYMS building renovation program, and this correlation must be clearly demonstrated in an application.

3. OFFICE OF PARKS, RECREATION & HISTORIC PRESERVATION - ENVIRONMENTAL PROTECTION FUND MUNICIPAL GRANT PROGRAM

Funding Available:

Up to \$15.5 Million

Description:

Funding is available under the EPF Mu-

nicipal Grant Program for the acquisition, planning, development, and improvement of parks, historic properties, and heritage areas located within the physical boundaries of the State of New York. Municipalities and not-for-profit organizations with an ownership interest in the property are eligible to apply.

Eligible types of applicants:

- Municipalities
- State Agencies
- Public Benefit Corporations
- Public Authorities
- Not-for-profit Corporations

Funding under the EPF Municipal Grant program is available for the following grant categories and project elements: Park Acquisition, Development and Planning Program - for the acquisition, development and planning of parks and recreational facilities to preserve, rehabilitate or restore lands, waters or structures for park, recreation or conservation purposes and for structural assessments and/or planning for such projects. Examples of eligible projects include: playgrounds, courts, rinks, community gardens, and facilities for swimming, boating, picnicking, hunt-

ing, fishing, camping or other recreational activities.

Historic Property Acquisition, Preservation and Planning Program - to improve, protect, preserve, rehabilitate, restore or acquire properties listed on the State or National Register of Historic Places and for structural assessments and/or planning for such projects.

Heritage Areas System Acquisition, Development and Planning Program - for projects to acquire, preserve, rehabilitate or restore lands, waters or structures, identified in the approved management plans for Heritage Areas designated under sections 35.03 and 35.05 of the Parks, Recreation and Historic Preservation Law, and for structural assessments or planning for such projects.

Additional resources: For more information, eligible applicants should contact the NYS Office of Parks, Recreation and Historic Preservation (OPRHP) Regional Grants Administrator for your county.

Visit <http://www.nysparks.com/grants>

4. NEW YORK STATE COUNCIL ON THE ARTS -ARTS, CULTURE AND HERITAGE PROJECTS

Funding Available:

Up to \$4 million

Description:

Funds for Local Assistance are available under Article 3 of NYS Arts and Cultural Affairs Law for the study of and presentation of the performing and fine arts; surveys and capital investments to encourage participation of the arts; to encourage public interest in the cultural heritage of the state; and to promote tourism by supporting arts and cultural projects.

Eligible applicants and activities:

NYSCA awards grants to non-profit organizations incorporated in New York State, New York State Indian tribes, and units of local New York State government.

Categories of support (Applicants may apply to only one of the following categories):

1. Arts, Culture and Heritage Projects
2. Artistic Program Capital Equipment (Building Related Capital Equipment Purchases and Improvements are not

- eligible for support)
3. Digital Film Projector Conversion Program
4. Arts Intern Workforce Development Program in Cooperation with the State University of New York (SUNY) and City University of New York (CUNY)

5. COMMUNITY DEVELOPMENT BLOCK GRANT PROGRAM

Funding Available:

Up to \$25 million

Description:

The Community Development Block Grant (CDBG) Program provides small communities and counties in New York State with a great opportunity to undertake activities that focus on community development needs such as creating or expanding job opportunities, providing safe affordable housing, and/or addressing local public infrastructure and public facilities issues.

Eligible Activities / Program

Benefit Requirements:

NYS CDBG applicants must address and resolve a specific community or economic development need within one

of the following areas: (1) Economic Development, (2) Small Business Assistance, (3) Public Infrastructure, or (4) Public Facilities.

Eligible Types of Applicants:

Eligible applicants are non-entitlement units of general local government (villages, cities, towns or counties), excluding metropolitan cities, urban counties and Indian Tribes that are designated entitlement communities. Non-entitlement areas are defined as cities, towns and villages with populations of less than 50,000, except those designated principal cities of Metropolitan Statistical Areas, and counties with populations of less than 200,000.

6. NEW YORK DEPARTMENT OF STATE - LOCAL WATERFRONT REVITALIZATION PROGRAM

Funding Available:

Up to \$12 million

Description:

The Local Waterfront Revitalization Program provides 50:50 matching grants on a competitive basis to revitalize communities and waterfronts. This is a reimbursement program.

Eligible Types of Applicants:

Eligible applicants are villages, towns, cities, and counties with the consent and on behalf of one or more town, village, or city located along New York's coasts or inland waterway designated pursuant to Executive Law, Article 42. A list of coastal water bodies and designated inland waterways is available at <http://www.dos.ny.gov/funding/>.

Applicants may also partner with other organizations; however, only applications from eligible applicants will be evaluated for funding, and all procurement requirements must be met.

Applicants working in partnership with neighboring municipalities to address regional land use, community development, and resource and/or environmental issues or opportunities are encouraged to apply.

Eligible Activities / Program

Benefit Requirements:

Local Waterfront Revitalization Program Grant Funding will be available for program planning, feasibility, design, or marketing of specific projects, construction projects, to advance the preparation or implementation of strategies for

community and waterfront revitalization through any of the following grant categories:

- Preparation or Implementation of a Local Waterfront Revitalization Program
- Redeveloping Hamlets, Downtowns and Urban Waterfronts
- Planning or Constructing Land and Water based Trails
- Preparing or Implementing a Lake-wide or Watershed Revitalization Plan

Additional Resources:

The Request for Applications is available on the Department of State's website: <http://www.dos.ny.gov/funding/>. Updates and/or modifications to the RFA, along with answers to written questions received, will be posted on the Department of State's website.

Pre-application workshops will be held at several locations around the State. The workshop schedule and further information are available at <http://www.dos.ny.gov/funding>. All those who would like to have the application process explained or have process-related questions are encouraged to attend.

For general questions on this grant program, please contact Kenneth Smith, New York State Department of State, Office of Communities and Waterfronts, 99 Washington Avenue, Albany, Suite 1010, New York 12231, call (518) 474-6000, email kenneth.smith@dos.ny.gov.

7. DEC Urban and Community Forestry Grant Program – Round 12

Funding Available:

\$2,500-50,000

Description:

The Department of Environmental Conservation is offering Municipal Governments, Public Benefit Corporations, Public Authorities, School Districts, and Not-For-Profit Corporations that have a public ownership interest in the property or are acting on behalf of a public property owner the opportunity to apply for Urban and Community Forestry Grants for projects throughout neighborhoods and parks to provide environmental, economic, and social benefits; and improved quality of life for New York urban residents.

Eligible project categories include tree

Funding Sources: Federal Grants

inventories; management plans (that require no match); tree planting; maintenance and invasive pest detection studies (require 50/50 match). Grant proposals should discuss the scope of work, how the project will provide environmental, economic and/or social benefits in the community. Proposals that help to implement watershed protection will also be considered.

Grant applications must be postmarked or delivered by 4:00 pm, December 5, 2013.

Applicants may obtain all necessary directions and forms for the Urban Forestry Grant on the DEC website: <http://www.dec.ny.gov/pubs/grants.html>

For more information, contact:

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1. TRANSPORTATION ENHANCEMENT PROGRAM (TEP)

The Transportation Enhancement Program (TEP) is a federal reimbursement program under the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU), administered by the New York Department of Transportation (NYSDOT).

In recognition that transportation systems are influenced and impacted by more than the condition of the traditional highway and bridge infrastructure, this program enables funding for transportation projects of cultural, aesthetic, historic and environmental significance.

TEP projects must fall into one or more of the following Federal Highway Administration (FHWA) categories to be eligible for funding:

1. Provision of Facilities for Bicycles and Pedestrians
2. Scenic and Historic* Highway Programs
3. Landscaping and Other Scenic Beautification
4. Preservation of Abandoned Railway

Corridors (Including Conversion and Use for Pedestrian and Bicycle Trails)

5. Environmental Mitigation to Address Water Pollution Due to Highway Run-off or Reduce Vehicle-caused Wildlife Mortality while Maintaining Habitat Connectivity

*Historic in the context of eligibility requires either inclusion of the National Registry of Historic Places or eligibility status for inclusion by the State Office of Historic Preservation.

Additionally, the project must have a transportation relationship with the surface transportation system and must be available for public access and use.

On May 2, 2013, New York State Department of Transportation Commissioner Joan McDonald announced that applications are being accepted for funding of transportation projects through the Transportation Enhancement Program (TEP).

Municipalities and not-for-profit groups are eligible to apply for funding. Applications submitted by not-for-profit organizations must be sponsored by a governmental entity.

Thirty million dollars will be available for

this application round.

Applications must be submitted to a NYSDOT Region on or by August 16, 2013. Awards will be announced by the end of the year.

This will be the last round of TEP, as the new surface transportation act, Moving Ahead for Progress in the 21st Century (MAP-21), ended the Transportation Enhancement Program as a standalone program.

2. MAP-21 (MOVING AHEAD FOR PROGRESS IN THE 21ST CENTURY)

MAP-21 is the successor to the federal surface transportation act, SAFETEA-LU, which expired on September 2009. It funds surface transportation programs in New York at approximately \$6 billion for fiscal years 2013 and 2014. MAP-21 has two programs that are a particular good fit with Complete Streets.

Highway Safety Improvement Program (HSIP)

Under MAP-21, funding for safety improvements has increased substantially. At NYSDOT, we continue to advance engineering solutions at intersections

and other locations with high numbers of pedestrian crashes. These improvements include sidewalk, street crossing/crosswalk, and/or shoulder improvements, pedestrian countdown timers, and other improvements. For more information on HSIP funds, contact the NYSDOT Regional Planning Office.

Transportation Alternatives Program

The Transportation Alternatives Program (TAP) provides funding for programs and projects defined as transportation alternatives, including on- and off-road pedestrian and bicycle facilities, infrastructure projects for improving non-driver access to public transportation and enhanced mobility, community improvement activities, and environmental mitigation; recreational trail program projects; Safe Routes to School projects; and projects for the planning, design or construction of boulevards and other roadways largely in the right-of-way of former Interstate System routes or other divided highways.

3. WATERSHED PROTECTION AND FLOOD PREVENTION (SMALL WATERSHEDS) GRANTS

The USDA Natural Resource Conservation Service (NRCS) provides funding to state and local agencies or nonprofit organizations authorized to carry out, maintain and operate watershed improvements involving less than 250,000 acres. The NRCS provides financial and technical assistance to eligible projects to improve watershed protection, flood prevention, sedimentation control, public water-based fish and wildlife enhancements, and recreation planning. The NRCS requires a 50-percent local match for public recreation, and fish and wildlife projects.

4. ECONOMIC DEVELOPMENT GRANTS FOR PUBLIC WORKS AND DEVELOPMENT OF FACILITIES

The U. S. Department of Commerce, Economic Development Administration (EDA), provides grants to states, counties and cities designated as redevelopment areas by EDA for public works projects that can include developing trails and greenway facilities. There is a 30-percent local match required, except in severely

distressed areas where federal contribution can reach 80 percent.

5. DESIGN ARTS PROGRAM

The National Endowment for the Arts provides grants to states and local agencies, individuals and nonprofit organizations for projects that incorporate urban design, historic preservation, planning, architecture, landscape architecture and other community improvement activities, including greenway development. Grants to organizations and agencies must be matched by a 50-percent local contribution. Agencies can receive up to \$50,000.

6. APPALACHIAN REGIONAL COMMISSION (ARC) AREA DEVELOPMENT GRANTS PROGRAM

The Appalachian Regional Commission (ARC) is a regional economic development agency that represents a partnership of federal, state, and local government. Established by an act of Congress in 1965, ARC is composed of the governors of the 13 Appalachian states and a federal co-chair, who is appointed by the president. Local participation is provided through multi-county local devel-

opment districts.

ARC funds projects that address the four goals identified in the Commission's strategic plan:

1. Increase job opportunities and per capita income in Appalachia to reach parity with the nation.
2. Strengthen the capacity of the people of Appalachia to compete in the global economy.
3. Develop and improve Appalachia's infrastructure to make the Region economically competitive.
4. Build the Appalachian Development Highway System to reduce Appalachia's isolation.

Each year ARC provides funding for several hundred projects in the Appalachian Region, in areas such as business development, education and job training, telecommunications, infrastructure, community development, housing, and transportation. These projects create thousands of new jobs; improve local water and sewer systems; increase school readiness; expand access to health care; assist local communities with strategic planning; and provide technical and managerial assistance to emerging businesses.

The following 14 New York counties are part of "Appalachia New York": Allegany, Broome, Cattaraugus, Chautauqua, Chemung, Chenango, Cortland, Delaware, Otsego, Schoharie, Schuyler, Steuben, Tioga, and Tompkins. The Southern Tier counties are each represented by one of three LDDs: Southern Tier West Regional Planning and Development Board (STW), based in Salamanca, NY; Southern Tier Central Regional Planning and Development Board (STC), in Corning, NY and Southern Tier East Regional Planning Development Board (STE), in Binghamton, NY.

Contact New York's Program Manager to request a preapplication package. The appropriate LDD may provide guidance on a project's eligibility for funding and assistance in preparing a grant application.

Applications due: July 8, 2013

7. FIVE STAR and URBAN WATERS RESTORATION PROGRAM

The Five Star and Urban Waters Restoration Program seeks to develop nation-wide-community stewardship of lo-

Funding Sources: Private Grants

cal natural resources, preserving these resources for future generations and enhancing habitat for local wildlife. Projects seek to address water quality issues in priority watersheds, such as erosion due to unstable streambanks, pollution from stormwater runoff, and degraded shorelines caused by development.

The program focuses on the stewardship and restoration of coastal, wetland and riparian ecosystems across the country. Its goal is to meet the conservation needs of important species and habitats, providing measurable and meaningful conservation and educational outcomes. The program requires the establishment and/or enhancement of diverse partnerships and an education/outreach component that will help shape and sustain behavior to achieve conservation goals. Funding priorities for this program include:

- o On-the-ground wetland, riparian, in-stream and/or coastal habitat restoration
- o Meaningful education and training activities, either through community outreach, participation and/or integration with K-12 environmental curriculum
- o Measurable ecological, educational and community benefits
- o Partnerships: Five Star projects

should engage a diverse group of community partners to achieve ecological and educational outcomes.

Applications can be accessed at www.nfwf.org/easygrants.

Urban Waters Project Elements

Competitive projects will include a focus on the following priorities:

- Urban Forest Restoration
- Education and Training
- Stormwater Management
- Monitoring
- Outdoor Recreation

More information available at www.urbanwaters.gov.

Applications due Feb. 5, 2014.

FUTURE for ONEONTA FOUNDATION

The Future for Oneonta Foundation was created by Dr. Frederic Fay Swift and subsequently organized to "Improve the quality of living for citizens of the Greater Oneonta area."

The Foundation's purposes, as stated in the By-laws, are to:

- Promote improvement of the quality of living and the general welfare of the community and the citizens of Greater Oneonta;
- Stimulate interest in the civic affairs of the community;
- Support the cultural and civic needs of the community; and
- Solicit donations from the public so as to accumulate sufficient funds to support the purposes of the Corporation.

FOF In Action:

Since its formation in 1982, FOF has provided more than \$400,000 in program support to area agencies and civic, cultural, and educational projects. Some of these include:

- Publishing a periodical, "Pride in One-

onta," on a regular basis.

- Supporting publications of brochures on the Oneonta area.
- Awarding scholarships to high school graduates.
- Hosting an annual luncheon for business, educational, professional, cultural and governmental leaders.
- The Foundation received its charter in the spring of 1982 and its 501(c)3 rating from the Internal Revenue Service in December 1982. Gifts and contributions made to the Foundation are tax deductible.